



BOARD OF ZONING APPEALS STAFF REPORT

To: Members of the Board of Zoning Appeals

From: Sergio Mendoza, Planning Director

Meeting Date: May 14, 2024

Agenda Item: BZA 24-001

Application Type: Developmental Standards Variances

Hearing: Public Hearing

Summary: Aaron Adelman with SMJ International, representing Tesla, is requesting a variance from Section 26-6.701.B.1. to permit twelve 18"W x 2.5"H double sided internally illuminated Tesla Logo signs on each of their Electric Vehicle (EV) charging stations located in the Target parking lot at 8005 Calumet Avenue.

Applicant: Aaron Adelman with SMJ International, representing Tesla

Property Address: 8005 Calumet Avenue.

Current Zoning: CD-4.A - General Urban-B Character District

Adjacent Zoning: North: CD-4.A - General Urban-B Character District
South: CD-3.R2 – Neighborhood 60' Lot One Family Residence
East: CD-4.A - General Urban-B Character District
West: CD-4.A - General Urban-B Character District

Action Requested: Motion to Approve

Actions Required: Motion to Open Public Hearing
Review Conditions of Approval Questions
Review of Zoning Code(s)

Staff Recommendation: Motion to Approve

Attachments:

1. Variance Application, Exhibit A (pg. 7)
2. Conditions of Approval Questions, Exhibit B (pg. 10)
3. Tesla Plans, Exhibit C (pg. 11)
4. EV Charging Station Data Sheet, Exhibit D (pg. 32)
5. BZA Minutes Excerpts, Exhibit E (pg. 34)
6. Previous Staff Reports regarding Tesla Use, Exhibit F (pg. 41)

PROJECT LOCATION

8005 Calumet Avenue, Target parking lot.

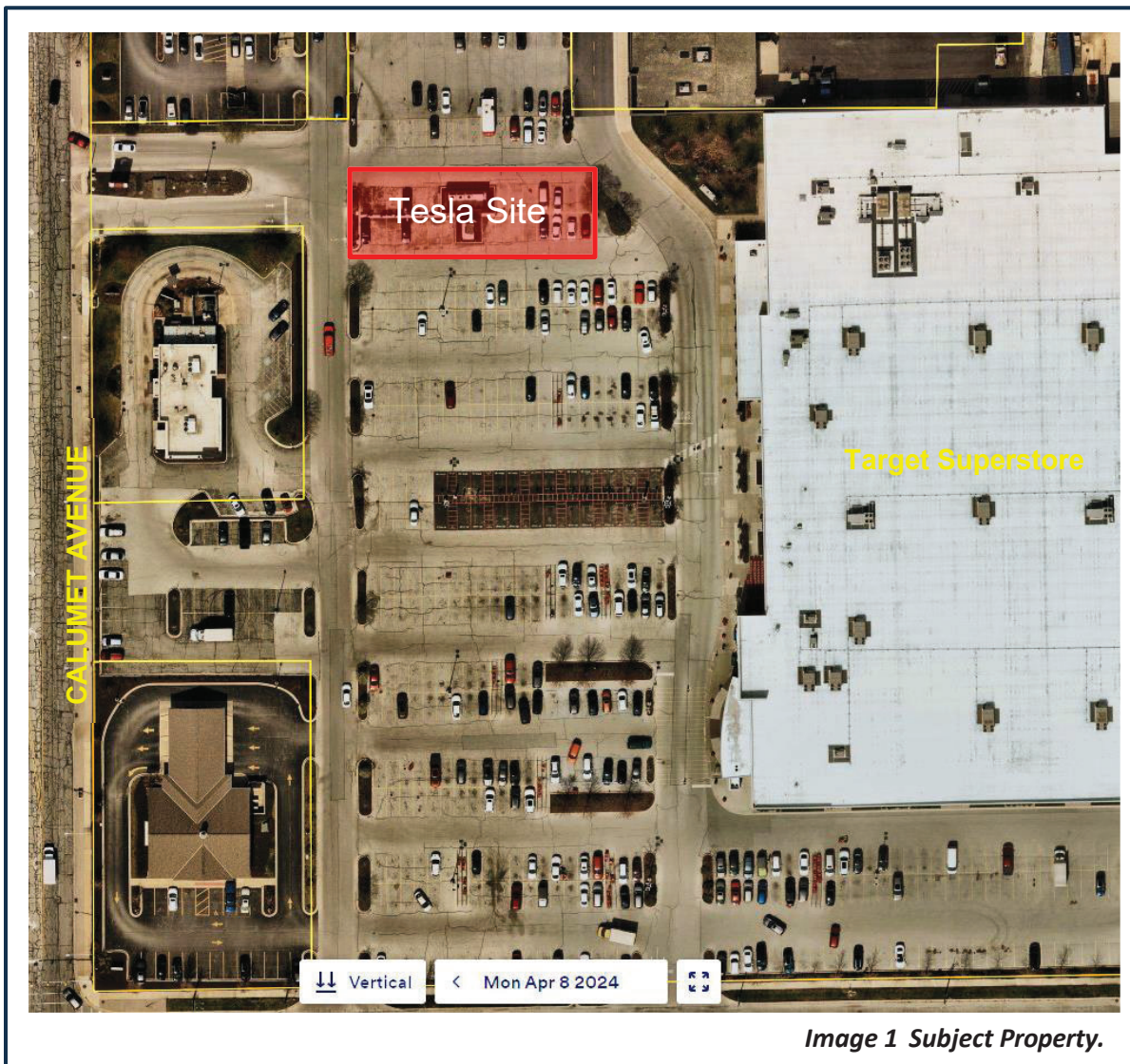


Image 1 Subject Property.

1005 Ridge Road • Munster, IN 46321 • (219) 836-8810 • Police/Fire Emergencies 911

Police Non-Emergency (219) 836-6600 • Fire Non-Emergency (219) 836-6960

www.munster.org

PROJECT BRIEFING

The use of the EV Charging Stations was discussed at the Board of Zoning Appeals on November 15, 2022, and March 14, 2023. At these meetings it was determined that EV charging stations and infrastructure are not defined in the Munster zoning ordinance and that EV charging stations are distinct in that common practice has been to install this types of uses within existing parking lots as an accessory use to a residential, commercial, civic, or manufacturing use and they are not typically installed as primary uses. The minutes of these meetings identify a catch-all provision which reads, “any accessory use that is customarily found in this district is permitted.” The Board concurred that this use meets that provision and that the EV Charging Station and should be considered an accessory use. Discussion ensued with respect to standards, it was advised that all development standards would be required to meet our zoning code of the primary use, *i.e.* minimum parking spaces, location of parking spaces, landscaping, site lighting, screening of utilities and infrastructure, and signage standards. Examples of standards were presented in the minutes and only signage standards identified for the purpose of informing the public of a certain size would be permitted.

Aaron Adelman with SMJ International, representing Tesla, is now requesting a Developmental Standards Variance because Tesla now finds a hardship in meeting Munster’s sign standards. The difficulty lies in that our zoning codes does not regulate signage for EV Charging Stations. The request is to allow twelve 1’-6” x 2.5” existing double sided internally illuminated Tesla Logo Signs on each of their 12 Electric Vehicle (EV) Charging Stations located in the Target parking lot at 8005 Calumet Avenue. Mr. Adelman has also stated the proposed size and internally illuminated Tesla logo signs are standard on all their EV Stations, see Image 2. Currently the only other signage, permitted under the sign standards, signs on bollards that inform the public, installation detail on page D-2 of the Project Data Sheet of the staff report and referenced on a site plan, see Image 3.



Day Logo/Sign



*Night non-illuminated Logo/Sign
Inactive charging station*



*Night Illuminated Logo/Sign
active charging station*

Image 2 Tesla Logo Signs

The Munster Character Based Zoning codes from which the petition is seeking a variances from is:

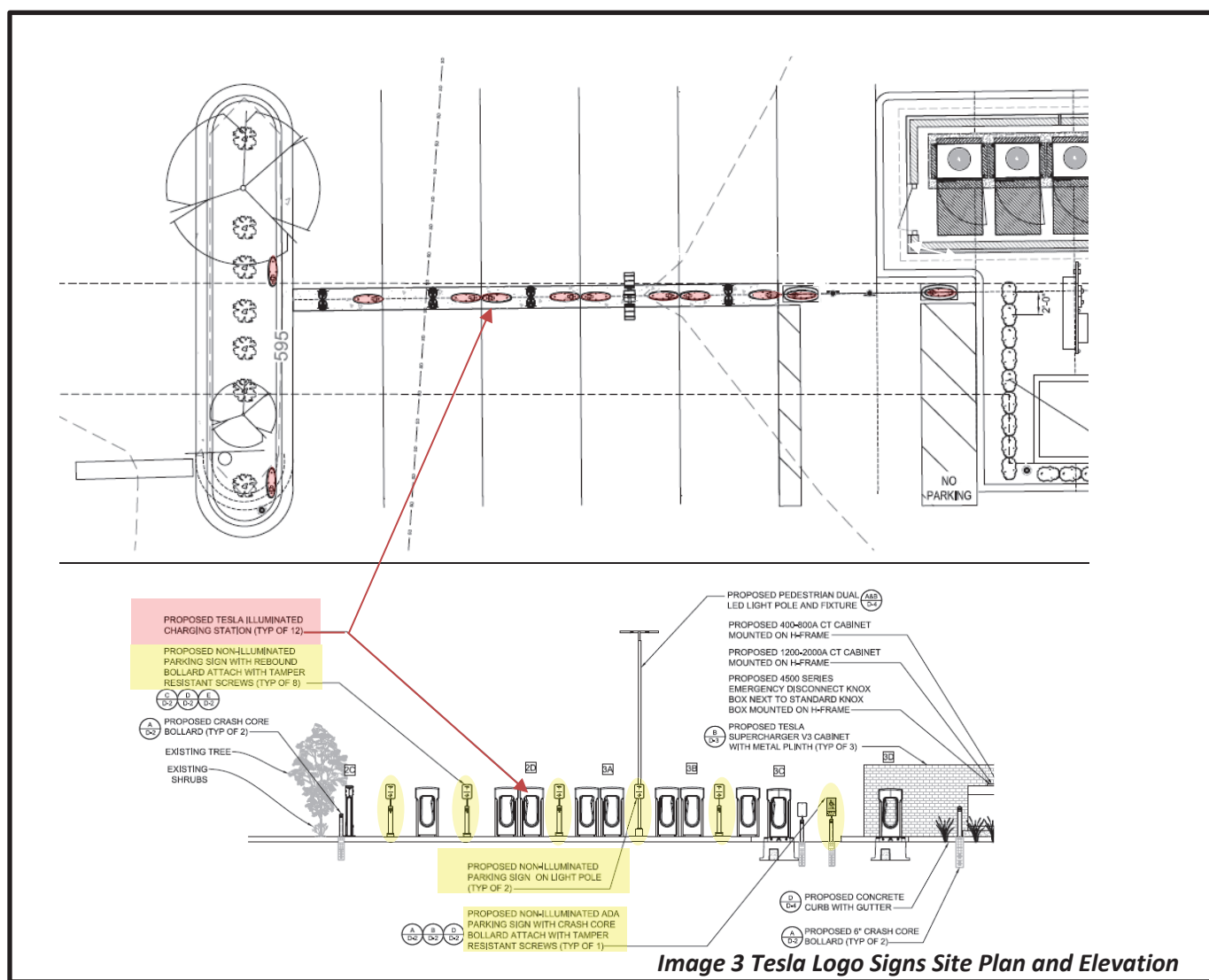
SECTION 26-6.701 SIGN STANDARDS, B. General Sign Provisions (MZC pg. 332)

REQUIRED:

1. There shall be no Signs allowed other than as specified in this Section 26-6.701.

PROPOSED:

Twelve (12), double sided internally illuminated Tesla logo signs, one for each EV Charging Station. Tesla logo sign dimensions and square footage information missing.



1005 Ridge Road • Munster, IN 46321 • (219) 836-8810 • Police/Fire Emergencies 911

Police Non-Emergency (219) 836-6600 • Fire Non-Emergency (219) 836-6960

www.munster.org

VARIANCE STANDARDS

The variance process is established to provide relief to a property owner when, due to unique circumstances, compliance with the zoning code imposes a hardship or practical difficulty on a property owner. The BZA is under no obligation to grant a variance. It is the petitioner's responsibility to prove a hardship or practical difficulty. The BZA should ask the petition to address the criteria listed below.

Section 26-6.804. I. Deviation from Standards & Requirements (pg 392) of the Munster Zoning Code states that the basis for a variance is as follows:

g. General Standards.

A Variance may be granted only if the Decision-Making Authority has made the following determinations for such Variance:

- i. the practical difficulties or unnecessary hardships that would be incurred by strict application of the Use or Development standard, as applicable, are unique and not shared by all properties in the vicinity and are not self-imposed;
- ii. such Variance is the minimum Variance that will relieve such practical difficulties or unnecessary hardships, as applicable;
- iii. such Variance is in the spirit of the general purposes and intent of this Article as stated in Division 1; and
- iv. such Variance is so designed as to provide reasonable consideration to, among other things, the character of the neighborhood, District, or Civic Zone, the conservation of property values in the vicinity, and the guidance of Development in accordance with the Comprehensive Plan.

h. Specific to Development standards Variances:

A Variance from Development Standards may be approved or approved with conditions only if:

- i. it will not be injurious to the public health, safety, morals, and general welfare of the community;
- ii. the use and value of the area Adjacent to the property included in the Variance will not be affected in a substantially adverse manner; and
- iii. the strict application of the Development standards will result in practical difficulties in the use of the property.

The applicant has addressed these criteria (Conditions of Approval Questions Form) in Exhibit B (page 10).

STAFF FINDINGS and RECOMMENDATION

Staff finds the applicant has complied with Public Notification requirements and on file the Certified Mailed Receipts and Proof of New Paper Publication in file BZA 24-001. Staff has also received confirmation of the proposed Tesla Logo Sign size (1'.6" x 2.5") and has advised the applicant of their failure to have representation at the Preliminary Hearing and reminded them of having presentation at the Public Hearing. Mr. Aaron Adelman with SMJ International, representing Tesla, has indicated that Tesla is experiencing some restructuring and there is uncertainty within the organization who will be represent this variance request at the Public Hearing.

In addition, staff also finds that the proposed twelve 18"W x 2.5"H double sided internally illuminated Tesla Logo Signs on each of their Electric Vehicle (EV) charging stations located in the Target parking lot at 8005 Calumet Avenue will not be injurious to the public health, safety, morals, and general welfare of the community and that the illuminated Tesla Logo Signs will actually alert the public when the charging station is operational. Staff also finds that the proposed Tesla Logo Signs will not adversely affect the value of the area adjacent to the property in a substantially adverse manner because the Tesla Logo Sign light luminosity is 108 lumens. At a distance of 20ft is 0.23 lux or 0.021-foot candle. Finally, staff finds that the lack of Development Standards in the Town's Zoning Codes for Charging Station signage will result in practical difficulties in the accessory use of the Tesla's charging station standard designs and would recommend approval.

MOTION

The Board of Zoning Appeals may consider the following motion:

Motion to APPROVE BZA 24-001, a Developmental Standards Variance for twelve 18"W x 2.5"H double sided internally illuminated Tesla Logo Signs on each of their Electric Vehicle (EV) charging stations, including all discussion and findings.



Exhibit A

Date: _____

Application Fee: \$ _____

Sign Fee: \$ _____

Town of Munster Board of Zoning Appeals Petition Application

OWNER INFORMATION:

Target Corporation
Name of Owner

PO Box 9456 Minneapolis MN 55440-9456
Street address, City, ST, ZIP Code

Phone Number

Email address

APPLICANT OR PETITIONER INFORMATION (if different than above):

SMJ International o/b/o Tesla (Aaron Adelman)
Name of Applicant/Petitioner

49030 Pontiac Trail, Suite 100, Wixom, MI 48393
Street address, City, ST, ZIP Code

616-916-3062
Phone Number

aadelman@smj-llc.com
Email address

PROPERTY INFORMATION:

Tesla Charging Station Installation
Business or Development Name (if applicable)

8005 Calumet Ave.
Address of Property or Legal Description

Current Zoning

APPLICATION INFORMATION:

Please select what this Application is for:

- ☒ **Variance** If yes, select one of the following: ☐ **Use** ☒ **Developmental Standards**
- ☐ **Conditional Use**
- ☐ **Administrative Appeal**

Brief Description of Project and List of Variances or Conditional Uses Being Requested (if applicable):

Tesla is seeking a variance from the following developmental standard: "Section 26-6.701.B.1. There shall be no Signs allowed other than as specified in this Section 26-6.701. Tesla has been approved to install 12 charging stations in the parking lot of the Target located at 8005 Calumet. As part of this installation, they will install a base charging station. On this charging station is the Tesla logo, when in operation, will illuminate. Tesla does not have the ability to install stations that do not have the logo nor do they have the ability to not illuminate the light. Understanding that the city does not want an abundance of signage, Tesla is agreeable to remove the other signs on site, should the board deem it necessary.

Peter Lichowski
Name of Registered Engineer, Architect or Land Surveyor

49030 Pontiac Trail, Suite 100, Wixom, MI 48393
Street address, City, ST, ZIP Code

Contact Applicant
Phone Number

Contact Applicant
Email address

REQUIRED ATTACHMENTS

Required Attachments for Board of Zoning Appeals Applications

To ensure that adequate information is provided to the BZA, please check off each of these items and provide documentation to the Community Development Department at the time of submittal of the application.

ALL APPLICATIONS	Included	N/A
Narrative statement describing project	X	
Property owner consent (Signature page)	X	
Proof of Ownership (e.g. copy of tax bill)		X
Plat of Survey depicting current conditions	X	
Site Plan containing the following:	X	
Boundary identification		
Fire hydrant locations		
Accessory structures		
Parking lot design		
Utility location		
Building footprints		
Proposed curb cuts		
Drainage/detention plans		
Traffic circulation		
Ingress/egress locations		
Major topographic information		
Infrastructure improvements		
Conditions of Approval Form (Note: complete the form specific to your petition)*		X
Any other information that the BZA may find useful in determining whether the application is merited.		

* Unique conditions have been established for special use permits for public garages, gas filling stations, used car lots, garden centers, massage parlors, adult bookstores, tattoo parlors, adult cabarets, and outdoor dining areas. Community Development staff will advise potential applicants of these at the pre-application meeting.

NOTE: If you checked any exhibits "N/A", please explain:

Tesla has an existing lease agreement with Target.



Petition BZA 24 - 001

Town of Munster Board of Zoning Appeals Application Signature Page

I hereby authorize SMJ International to act on my behalf as my agent in this petition and to furnish, upon request, supplemental information in support of this petition application.

Iryna Hanyshyn Tesla 02/27/24
Signature of Owner **Date**

Aaron Adelman 2/27/24
of SMJ International
Signature of Applicant **Date**

Exhibit B

BZA 24-001

DEVELOPMENTAL VARIANCE CONDITIONS OF APPROVAL

The Munster Board of Zoning Appeals is authorized to hear petitions for developmental standards variances and to approve or deny. The Board of Zoning Appeals may also impose reasonable conditions and restrictions. Indiana Code 36-7-4-918.5 lists the legal criteria for a developmental standards variance:

1. The approval will not be injurious to the public health, safety, morals, and general welfare of the community. Explain why this statement is true in this case:

The approval of this variance will not be injurious to the community in any of the ways mentioned. The lighting of the equipment will serve to ensure that the public knows the machines are operational and available for use. The illumination of the equipment will not encroach on the existing and proposed lighting meant to illuminate the parking lot.

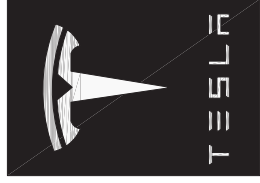
2. The use and value of the area adjacent to the property included in the variance will not be affected in a substantially adverse manner. Explain why this statement is true in this case:

The use will not be affected in an adverse manner. The approval of this variance will serve to improve the use and value of the area. The equipment will not function without the illuminated sign as it is part of the base charging station.

3. The strict application of the terms of the zoning ordinance will result in practical difficulties in the use of the property. Explain why this statement is true in this case:

Through the strict application of the terms of the zoning ordinance, Tesla is unable to put this location into operation. Unfortunately, the equipment with the illuminated sign is a critical part of the design of the charging station and cannot be altered (i.e. not lit or have the logo removed). Tesla understands the city's concerns with regard to an overabundance of signage. As such, Tesla is agreeable to removing other signage, if the board deems it necessary.

Attach additional pages if necessary



TESLA

SUPERCHARGER STATION

SITE NAME: MUNSTER, IN (TRT: 18681)

TARGET STORE # T-1913:

8005 CALUMET AVE

MUNSTER, IN 46321

PAGE 11

EXHIBIT C



3500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 681-5000

LAB

49030 Pontiac Trail, Suite 100
Wixom, Michigan 48393
PHONE: 248-705-9212

DRAWN BY: RC
CHECKED BY: PL

REV	DATE	DESCRIPTION
E	07/21/2021	CD100
D	04/11/2021	CD100
C	10/01/2021	CD100
B	06/11/2021	CD100
A	06/27/2021	CD09



SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

SHEET TITLE
TITLE SHEET &
PROJECT DATA

SHEET NUMBER
T-1

SHEET NO:	TITLE SHEET & PROJECT DATA
T-1	TOPOGRAPHIC SURVEY
A-1	OVERALL SITE PLAN
A-2	DEMOLITION SITE PLAN
A-3	PROPOSED SITE PLAN
A-4	TREE ENCLOSURE DETAILS
A-5	LANDSCAPING PLAN
S-1	MASONRY ENCLOSURE DETAILS
S-2	MASONRY ENCLOSURE DETAILS
E-1	ELECTRICAL PLAN
E-2	ELECTRICAL PLAN & PANEL SCHEDULE 'MDP'
E-3	SYSTEM ONE-LINE & V3 SUPERCHARGER INTERCONNECTION DIAGRAM
E-4	CHARGEPOINT SYSTEM
E-5	PLACARDS AND LABELS
G-1	GROUNDING DETAILS
D-1	INSTALLATION DETAILS
D-2	INSTALLATION DETAILS
D-3	INSTALLATION DETAILS
D-4	INSTALLATION DETAILS
D-5	INSTALLATION DETAILS

ARCHITECT OF RECORD
PETER LICHOWSKI ARCHITECT
49030 PONTIAC TRAIL, SUITE 100
WIXOM, MI 48393
(248) 705-9212
peter@lichowski.com

PROJECT ENGINEER
CLARK TROMBLEY RANDERS
904 S. CREYTS RD, SUITE B
LANSING, MI 48917
(517) 886-0550
rtrombley@ctrmep.com

CALL BEFORE YOU DIG
IF YOU DIG IN ANY STATE
DIAL 811 FOR THE LOCAL
"ONE CALL CENTER" -
IT'S THE LAW

THE UTILITIES SHOWN HEREIN ARE FOR THE CONTRACTOR'S CONVENIENCE ONLY. THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO ANY EXCAVATION. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE USE OF THIS INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR THE PROTECTION OF ALL UTILITIES. NO LIABILITY SHALL BE ASSUMED BY THE ENGINEER OR ARCHITECT FOR ANY DAMAGE TO OR LOSS OF ANY UTILITIES OR OTHER PROPERTY RESULTING FROM THE USE OF THIS INFORMATION.

ZONING INFORMATION
PERMITTING: TOWN OF MUNSTER IN
JURISDICTION: THOMAS VANDER WOUDE
PLANNING DIRECTOR
t.vanderwoude@munster.org
(219) 336-6585

DO NOT SCALE DRAWINGS
CONTRACTOR SHALL VERIFY ALL PLANS. EXISTING UTILITIES SHALL BE LOCATED AND DEPTHS SHALL IMMEDIATELY NOTIFY THE ARCHITECT / ENGINEER IN WRITING OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK.

PROJECT DESCRIPTION
• INSTALL (3) TESLA SUPERCHARGER CABINETS
• INSTALL (12) TESLA CHARGING STATIONS
• INSTALL (1) UTILITY TRANSFORMER
• INSTALL (1) SWITCHGEAR ASSEMBLY
• INSTALL (1) MASONRY ENCLOSURE
• INSTALL (2) CHARGEPOINT LEVEL 2 CT4021 DUAL PORT CHARGERS
• INSTALL (1) H-FRAME MOUNTED CT CABINET
• INSTALL (1) H-FRAME MOUNTED KNOX BOX

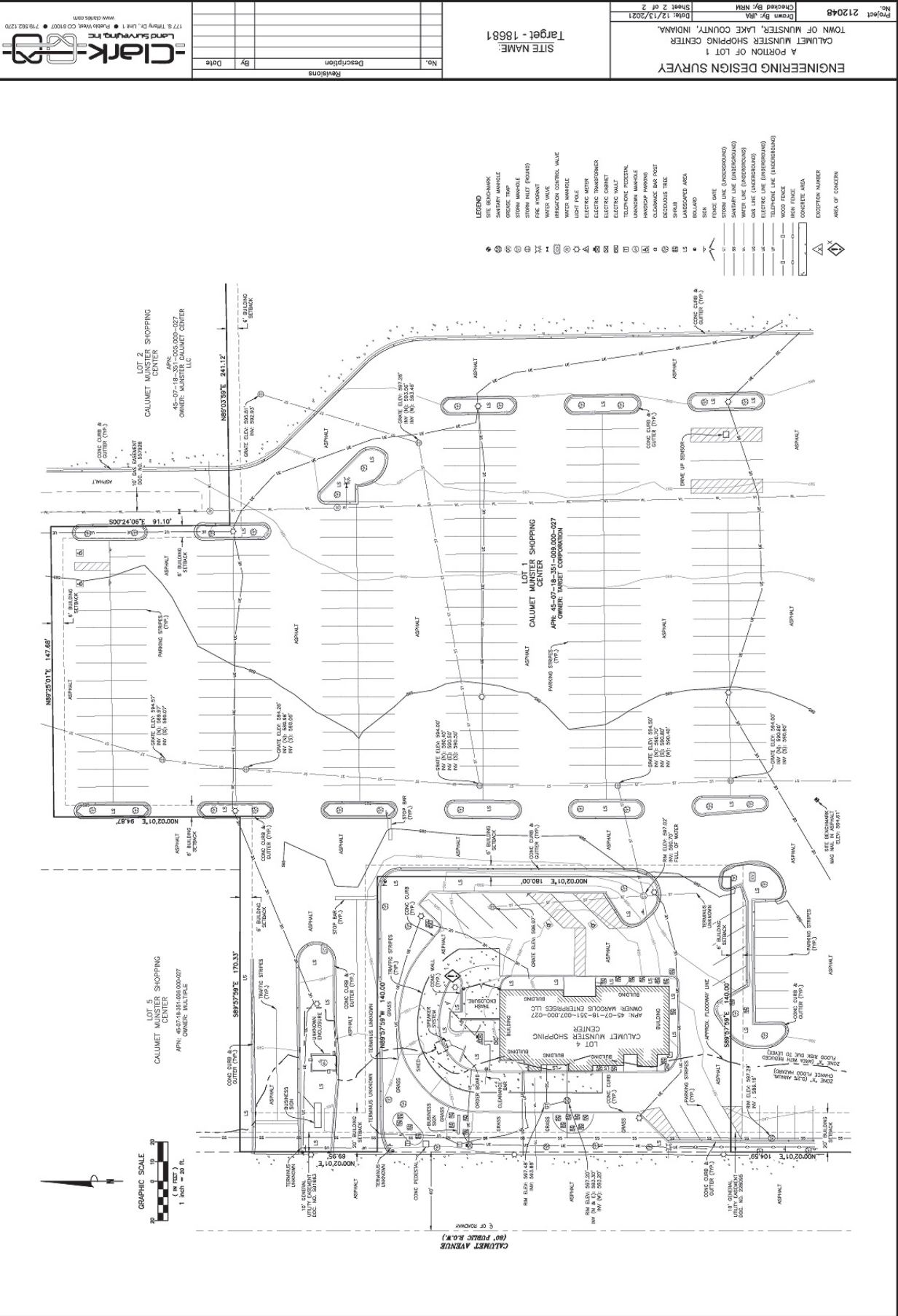
APPLICABLE CODES
ALL WORK SHALL COMPLY WITH THE FOLLOWING APPLICABLE CODES:
2012 INTERNATIONAL BUILDING CODE (1ST PRINTING), WITH 2014 INDIANA AMENDMENTS
2008 NATIONAL ELECTRICAL CODE (1ST PRINTING), WITH INDIANA AMENDMENTS
IN THE EVENT OF CONFLICT, THE MOST RESTRICTIVE CODE SHALL PREVAIL

SITE INFORMATION
PROPOSED TESLA EV SITE ADDRESS:
MUNSTER, IN 46321
EXISTING SITE ADDRESS:
8005 CALUMET AVE
MUNSTER, IN 46321
PROPERTY OWNER:
CHRIS ARMSTRONG
PROJECT MANAGER
EQUIPMENT SUPPLIER:
TESLA MOTORS, INC.
3500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 681-5000
POWER COMPANY:
NIPSCO
LAKE
LATITUDE (NAD83):
41° 34' 04.7" N
41° 56' 58.2"
LONGITUDE (NAD83):
87° 30' 29.4" W
-87.508153°



FLOOD HAZARD AREA NOTE
THIS SITE IS LOCATED IN FLOOD ZONE "X".
NO BASE FLOOD ELEVATION.
AREA DETERMINED TO BE OUTSIDE 500-YEAR FLOOD PLAIN.

CONTRACTOR NOTE
CONTRACTOR SHALL COMPLETE INSTALL PER ALL APPLICABLE CODES AND ALL NECESSARY DEVIATIONS FROM THE U.S. DRAWINGS MUST BE SUBMITTED THROUGH AN RFI REQUEST PROCESS WITH ARCHITECT / ENGINEER FOR AN APPROVAL PRIOR TO CONTRACTOR PROCEEDING WITH A CONSTRUCTION OF THE SIGNED AND SEALED SET OF DRAWINGS.





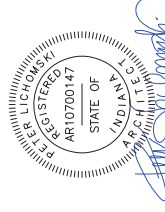
3500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 797-5000



49030 Pontite Trail, Suite 100
Livonia, Michigan 48333
PHONE: 248-705-9212

DRAWN BY:	RC
CHECKED BY:	PL

REV	DATE	DESCRIPTION
E	07/21/2021	CD100
D	04/11/2021	CD100
C	10/01/2021	CD100
B	06/11/2021	CD100
A	06/27/2021	CD09

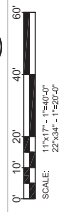
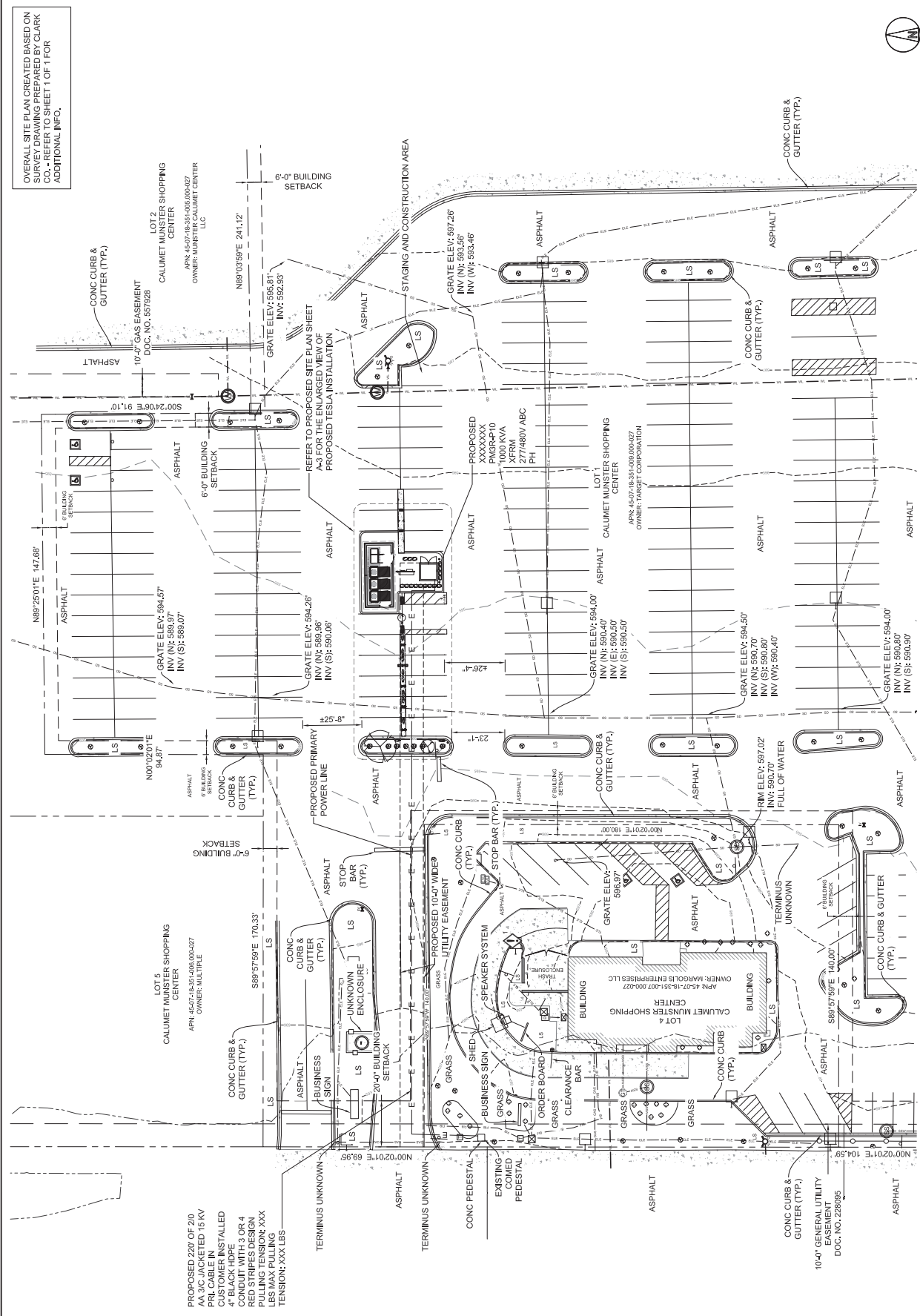


SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

OVERALL SITE PLAN

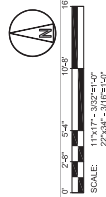
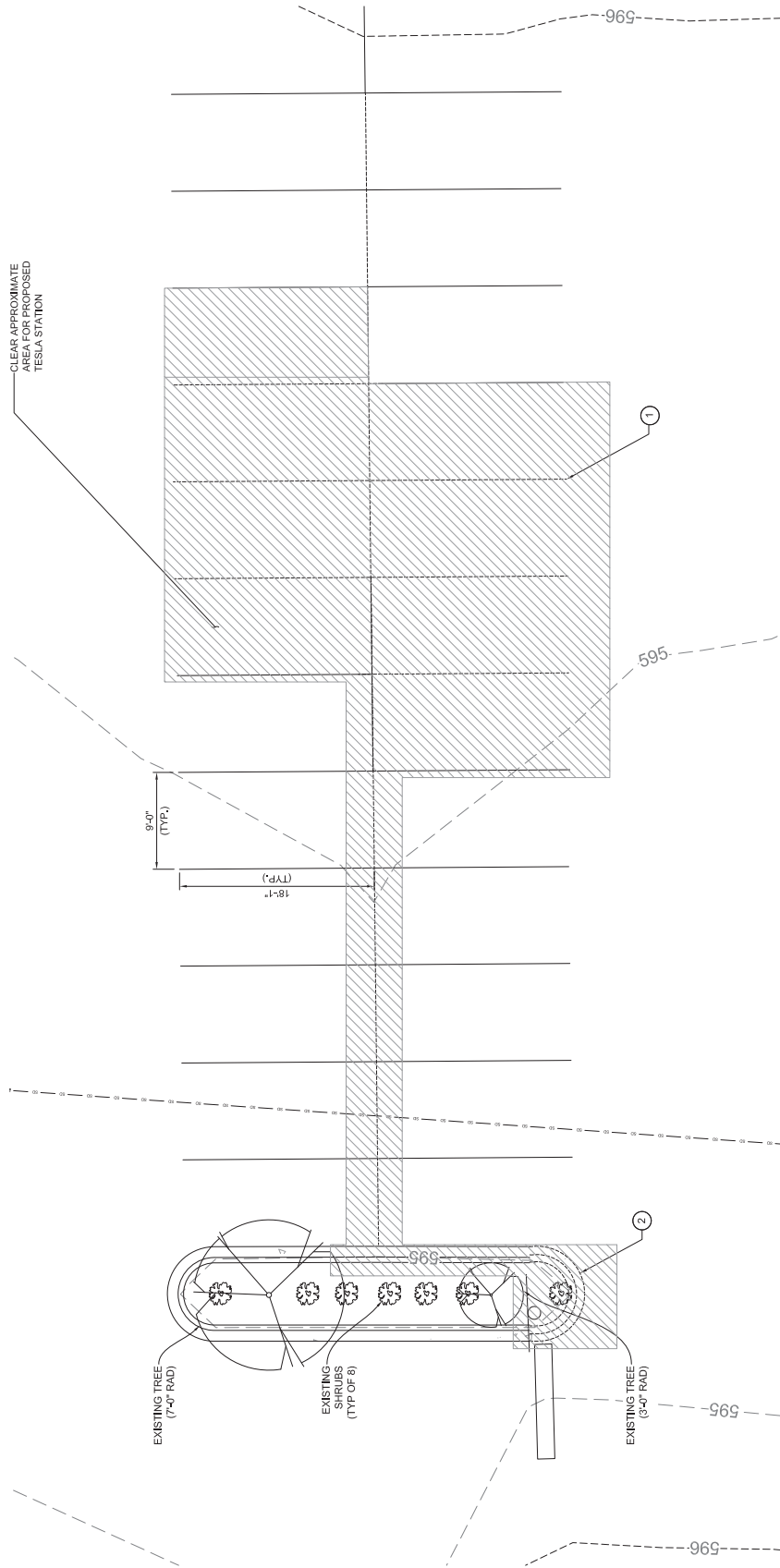
SHEET NUMBER
A-1

OVERALL SITE PLAN CREATED BASED ON
SURVEY DRAWING PREPARED BY CLARK
CO., REFER TO SHEET 1 OF 1 FOR
ADDITIONAL INFO.



OVERALL SITE PLAN

DEMOLITION SCOPE OF WORK	
1	EXISTING PARKING STRIPES TO BE REMOVED (TYP OF 5)
2	EXISTING CURB AND GUTTER TO BE REMOVED AND REPLACED



DEMOLITION PLAN



TESLA


3500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 997-2660

LAB

49030 Pontiac Trail, Suite 100
Wixom, Michigan 48393
PHONE: 248-705-9212

DRAWN BY:	RC
CHECKED BY:	PL

REV	DATE	DESCRIPTION
E	07/21/2022	CD100
D	04/11/2022	CD100
C	10/01/2022	CD100
B	06/11/2022	CD100
A	06/27/2022	CD00



SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

SHEET TITLE
DEMOLITION SITE PLAN

SHEET NUMBER
A-2

3500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 681-5000



49030 Pontiac Trail, Suite 100
Wixom, Michigan 48393
PHONE: 248-705-9212

	RC	PL
DRAWN BY:		
CHECKED BY:		

REV	DATE	DESCRIPTION
A	05/27/2022	CD90
B	06/11/2023	CD 100
C	10/01/2022	CD 100
D	04/11/2023	CD 100
E	07/31/2023	CD 100



SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

PROPOSED SITE PLAN

A-3
SHEET NUMBER

NOTES

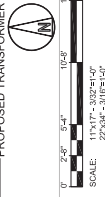
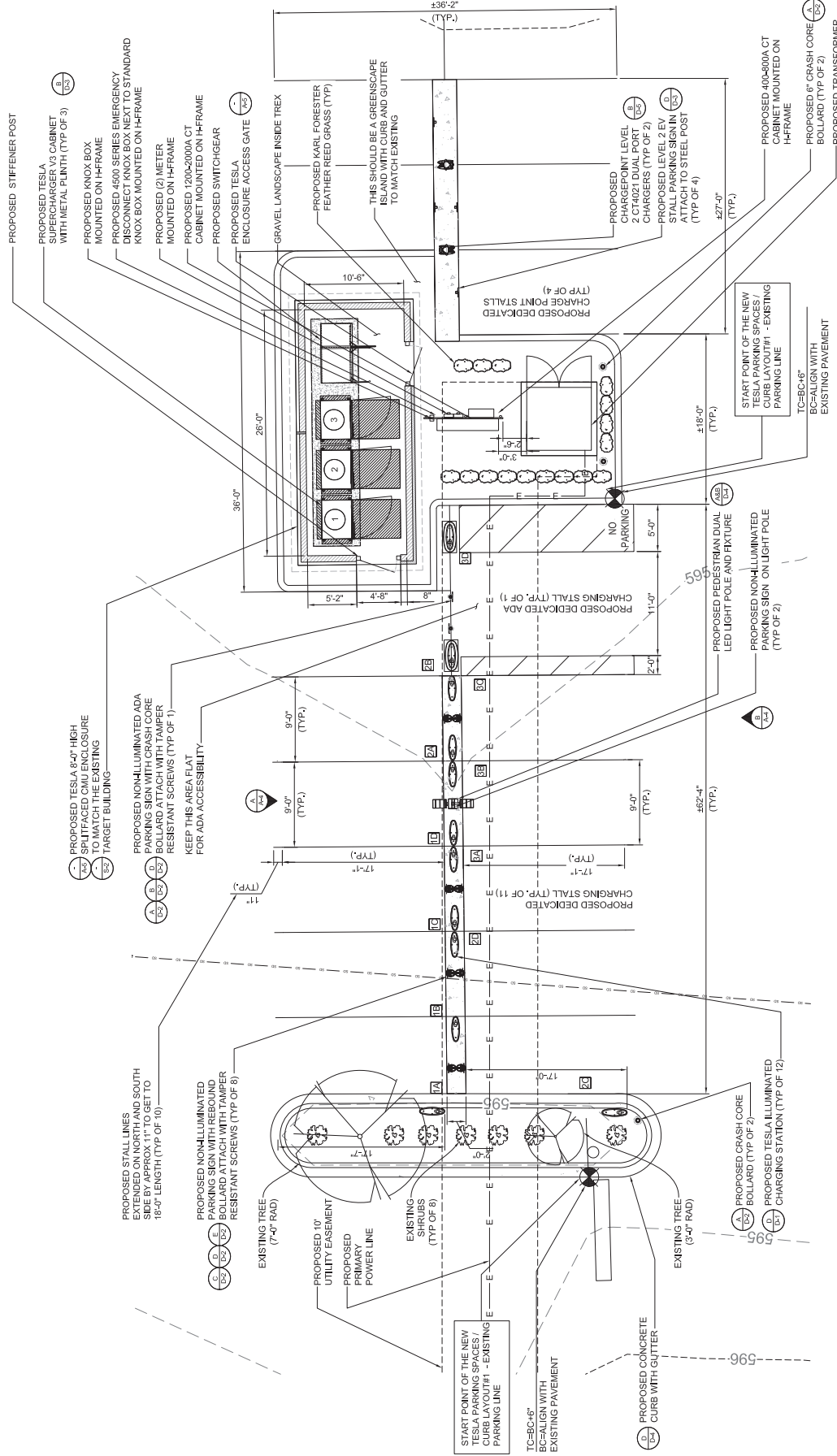
1. CONTRACTOR SHALL RETURN ALL DISTURBED AREAS OF PROPERTY TO ORIGINAL SITE CONDITION.
2. THE CONTRACTOR SHALL VERIFY ALL DEMOLITION AND CONSTRUCTION QUANTITIES PRIOR TO BIDDING.
3. CONTRACTOR SHALL BE RESPONSIBLE FOR SEAL COAT AND RESTRAINING OF STALL AREA UPON COMPLETION OF WORK, WHERE APPLICABLE. GO TO VERIFY ALL DEPTHS, DIMENSIONS AND SQUARE FOOTAGE WITH TESLA CA BEFORE CONSTRUCTION.

PROJECT AREA STALL COUNT

EXISTING STALL COUNT	35
PROPOSED TESLA CHARGING STALLS	12
ADDITIONAL STALLS TO BE REMOVED	6
TOTAL PROPOSED STALL COUNT	29

TESLA EQUIPMENT SCHEDULE

TESLA EQUIPMENT	DESCRIPTION	PART NUMBER	QUANTITY
SUPERCHARGER CABINETS	VERSION 3	1450739-00-D	3
CHARGING POST	VERSION 3	1088595-00-D	12



PROPOSED SITE PLAN



5500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 681-5000

49030 Pontiac Trail, Suite 100
Wixom, Michigan 48393
PHONE: 248-705-9212

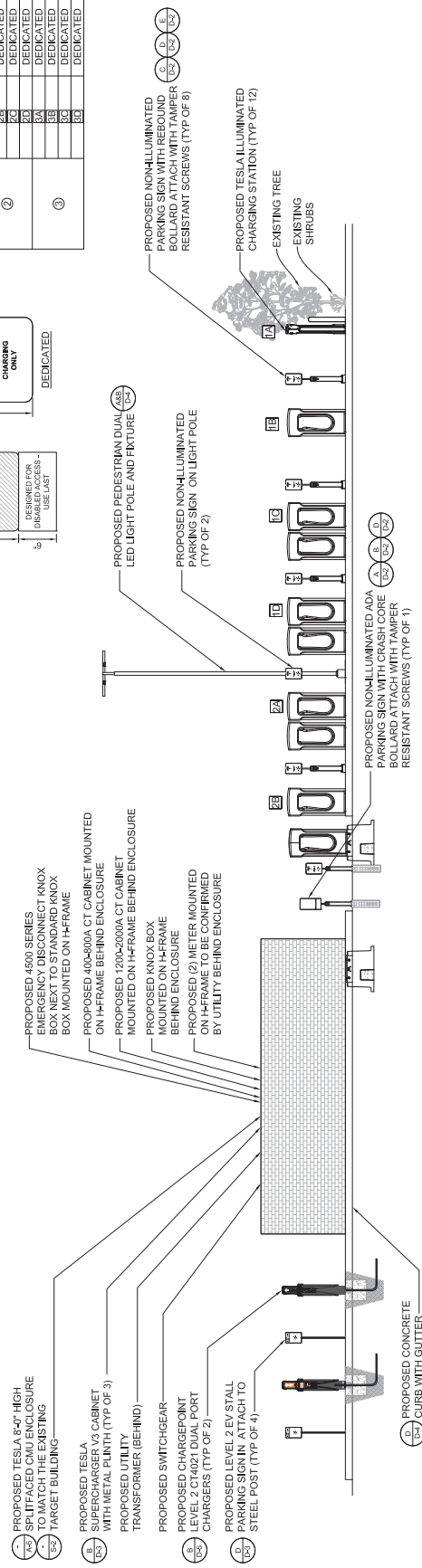
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	D	04/11/2023	CD100	
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	A	05/27/2022	CD50	
	REV	DATE	DESCRIPTION	



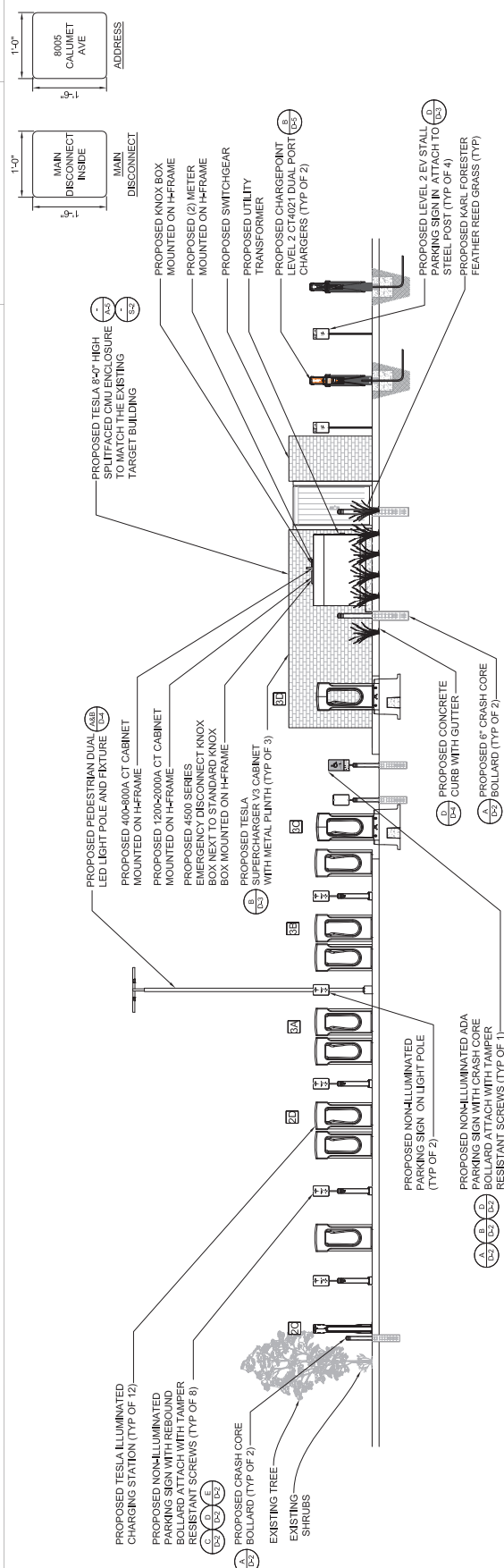
SHEET TITLE

A-4

1. BOTTOM OF LOWEST SIGN TO BE INSTALLED 54" ABOVE GRADE.
2. ADDITIONAL PARKING SIGNS TO BE INSTALLED 2' ABOVE TOP OF PREVIOUS SIGN.
3. DO NOT ANCHOR SIGNS OR PENETRATE SIDE OF SWITCHGEAR ASSEMBLY.
4. SIGNAGE TO BE REFLECTIVE WHITE.
5. LETTERING ON WHITE BACKGROUND.
6. ALL TEXT SHALL BE CAPITAL LETTERS. LARGER FONT SHALL BE ARIAL OR SIMILAR AND IS NOT TO BE BOLD.
7. CONTRAST BETWEEN CHARACTERS, SYMBOLS AND THEIR BACKGROUND SHALL BE 70% MINIMUM AND HAVE A NON GLARE FINISH.



SITE ELEVATION



SITE ELEVATION



49030 Pontiac Trail, Suite 100
Wixom, Michigan 48393
PHONE: 248-705-9212

DRAWN BY:	RC
CHECKED BY:	PL

REV	DATE	DESCRIPTION
A	05/27/2022	CD50
B	06/11/2022	CD100
C	10/01/2022	CD100
D	04/11/2023	CD100
E	07/31/2023	CD100



SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

SHEET TITLE

SHEET NUMBER

A-5



TREX 5"X5" FENCE POST
(TYPICAL). SEE SHEET A-6
AND S-2 FOR INSTALLATION

PROPOSED	GRADE
----------	-------

1. **INSTALLATION TO BE COMPLETED PER MANUFACTURER'S RECOMMENDATIONS.**

2. THIS DRAWING IS PROVIDED FOR PLANNING PURPOSES. REFER TO MANUFACTURER'S RECOMMENDATIONS FOR CONSTRUCTION DETAILS.
3. REFER TO MANUFACTURER'S WEBSITE FOR PRODUCT INFORMATION.
4. CONTRACTOR SHALL ENSURE GATE JAMB WIDTH AND PLACEMENT IS SUITABLE FOR GATE HARDWARE ON DETAIL A, THIS SHEET.

1. FOR SOLID GATES & DOORS, CONTRACTOR SHALL UTILIZE LOCKEY STYLE 285P MED DUTY LEVER WITH PANIC BAR OR APPROVED EQUAL, PURCHASE ADDITIONAL ACCESSORIES AS REQUIRED.
2. FOR CHAINLINK GATES, CONTRACTOR SHALL UTILIZE LOCKEY/PS11-SECURITY KIT OR APPROVED EQUAL.

2. FOR CHAINLINK GATES, CONTRACTOR SHALL UTILIZE LOCKEY PS11-SECURITY KIT OR APPROVED EQUAL

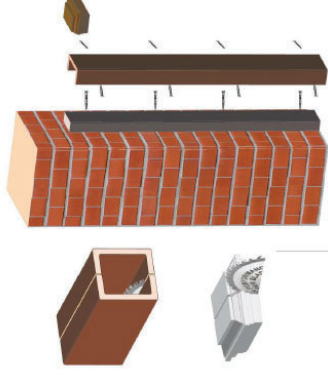
TREX FENCE DETAIL

NO SCALE	A
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	B
NO SCALE	

Trex® Seclusions®
COMPOSITE FENCING SYSTEM

Pillar Mount - Post Attachment



1. CONTRACTOR SHALL ENSURE GATE JAMB WIDTH AND PLACEMENT IS SUITABLE FOR GATE HARDWARE ON DETAIL A, SHEET A-5.

2. SEE PILLAR CONNECTION DETAIL ON S-2.

TREX GATE ATTACHMENT TO MASONRY WALL

D	NO SCALE
---	----------

TE

GENERAL NOTE

1. ALL PLANTS SHALL BE WATERED THOROUGHLY TWICE DURING THE FIRST 24 HOUR PERIOD AFTER PLANTING. CONTRACTOR TO PROVIDE WATERING PLAN FOR THE FIRST GROWING SEASON.
2. PLANTINGS SHALL BE LEVEL OR SLIGHTLY BELOW THE ISLAND'S EDGE TO FACILITATE RAIN WATER COLLECTION FOR THE PLANTS AND TO KEEP ALL PLANT MATERIALS (MULCH, STONES, ETC.) IN THE ISLAND.

PLANT SCHEDULE					
CODE	BOTANICAL NAME	COMMON NAME	QTY	SIZE (AT CONSTRUCTION)	SPECIFICATIONS (AT FULL SIZE)
ORNAMENTAL GRASSES	CALAMAGROSIS ACUTIFLORA	KARL FORESTER FEATHER REED GRASS	-	4'-18" TALL	4'-6" TALL, 2'-3" WIDE



3500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 797-5000

LAB

49030 Pontiac Trail, Suite 100
Waconia, Michigan 48383
PHONE: 248-705-9212

DRAWN BY:	RC
CHECKED BY:	PL

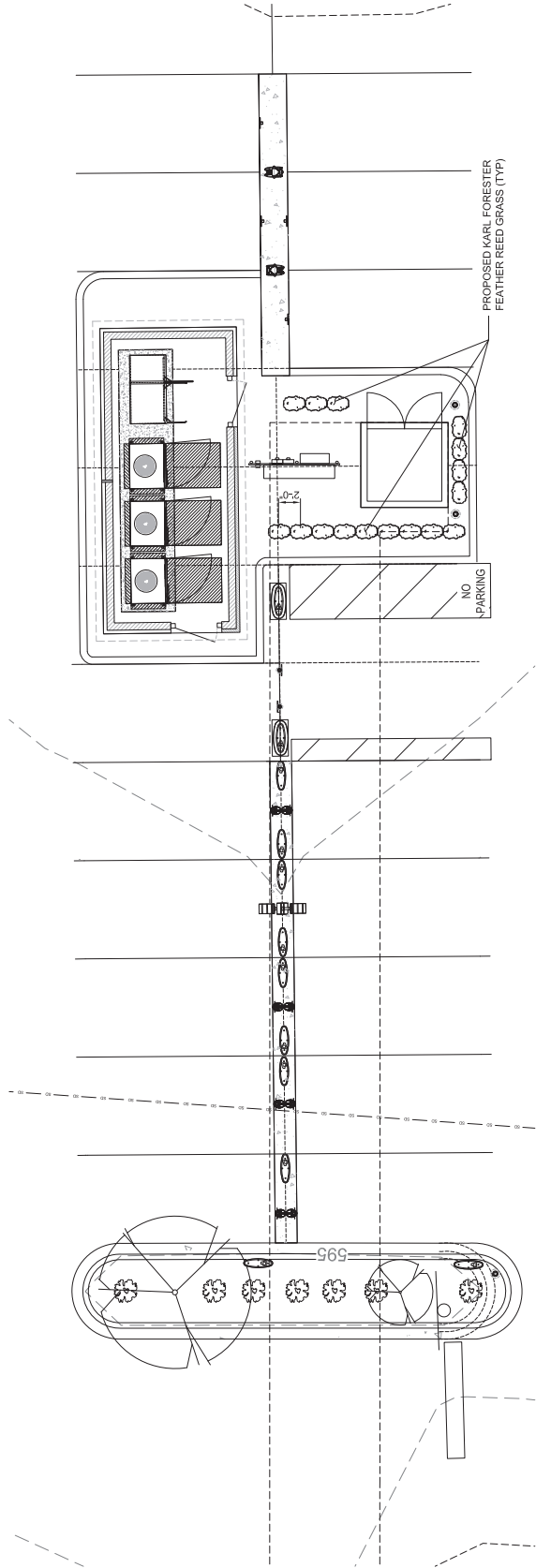
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D	04/11/2022	CD100
C	10/01/2022	CD100
B	06/11/2022	CD100
A	06/27/2022	CD00



SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

SHEET TITLE
LANDSCAPING PLAN

SHEET NUMBER
LS-1





49030 Pontiac Trail, Suite 100
Wixom, Michigan 48393
PHONE: 248-705-9212

RC	
PL	
DRAWN BY:	
CHECKED BY:	

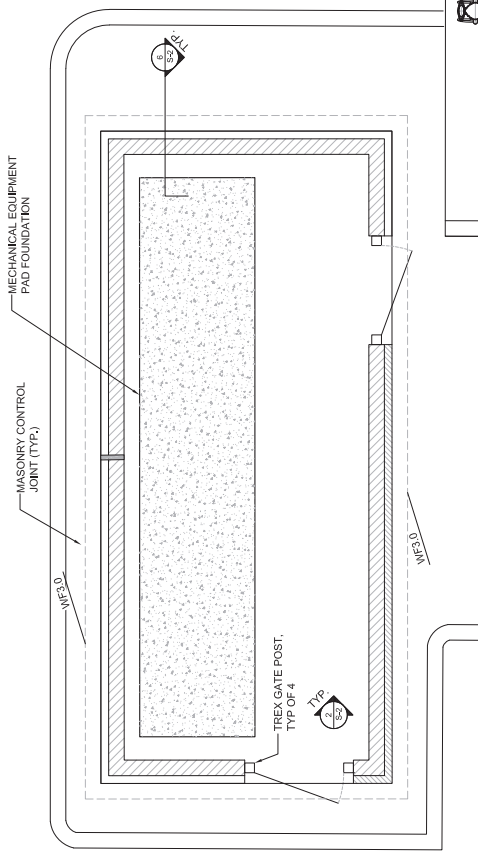
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B	06/11/2022	CD100
C	10/01/2022	CD100
D	04/11/2023	CD100
E	07/31/2023	CD100



SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

SHEET TITLE
MASONRY
ENCLOSURE DETAILS

S-1



NOTE:

- SEE B/A-5 FOR GATE SPECIFICATIONS

ENCLOSURE PLAN

1. CONCRETE MASONRY UNITS SHALL BE MEDIUM WEIGHT UNITS CONFORMING TO ASTM C90, GRADE N-1, (F'W=2,000 PSI, BULK WEIGHT, (115 PCF)
2. MORTAR SHALL BE TYPE "S" ABOVE GRADE CONFORMING TO ASTM C270, (MINIMUM 1,800 PSI AT 28 DAYS)
3. GROUT SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 2,000 PSI AT 28 DAYS CONFORMING TO ASTM C476.
4. MASONRY SHALL BE Laid IN RUNNING BOND.
5. ALL VERTICAL REINFORCING LAP SPICES SHALL BE 48 BAR DIAMETERS.
6. ALL CELLS CONTAINING REINFORCING STEEL OR EMBEDDED ITEMS AND ALL CELLS IN RETAINING WALLS AND WALLS BELOW GRADE SHALL BE SOLID GROUTED.
7. ALL HORIZONTAL REINFORCING (BOND BEAMS) SHALL BE PLACED IN REBAR BLOCK UNITS.
8. PROVIDE HORIZONTAL LADDER STYLE JOINT REINFORCING WITH 9 GAGE SIDE AND GROSS RODS (GALVANIZED) SPACED AT 16" ON CENTER VERTICALLY, HORIZONTAL JOINT REINFORCING SHALL BE LAPPED A MINIMUM OF 12" CROSS BARS OR 6" WHICHEVER IS GREATER.
9. WHEN GROUTING IS STOPPED FOR ONE HOUR OR LONGER, HORIZONTAL CONSTRUCTION JOINTS SHALL BE FORMED BY STOPPING THE GROUT POUR 1-1/2" BELOW TOP OF THE UPPERMOST UNIT.
10. ALL BOND BEAM BLOCK SHALL BE "DEEP CUT" UNITS.
11. PROVIDE INSPECTION AND CLEANOUT HOLES AT BASE OF VERTICAL CELL, HAVING GROUT LIFTS IN EXCESS OF 4'-9" OF HEIGHT.
12. ALL GROUT SHALL BE CONSOLIDATED WITH A MECHANICAL VIBRATOR INSURE STRENGTH OF 2,000 PSI AT 28 DAYS CONFORMING TO ASTM C476.
13. REINFORCING BARS - SEE NOTES UNDER "REINFORCED CONCRETE NOTES" FOR REQUIREMENTS. REINFORCEMENT SHALL BE PLACED PRIOR TO GROUTING.
14. PROVIDE ONE BAR DIAMETER (A MINIMUM OF 1/2") GROUT BETWEEN MAIN REINFORCING AND MASONRY UNITS.
15. LOW LIFT CONSTRUCTION, MAXIMUM GROUT POUR HEIGHT IS 4 FEET.
16. HIGH LIFT GROUTED CONSTRUCTION MAY BE USED IN CONFORMANCE WITH PROJECT SPECIFICATIONS AND SECTION 2104 OF BIC.
17. ALL CELLS IN CONCRETE BLOCKS WITH VERTICAL REINFORCING SHALL BE FILLED SOLID WITH GROUT, EXCEPT AS NOTED IN THE DRAWINGS OR SPECIFICATIONS.
18. CELLS SHALL BE IN VERTICAL ALIGNMENT. DOWELS IN FOOTINGS SHALL BE SET TO ALIGN WITH CELLS CONTAINING REINFORCING STEEL.
19. REFER TO DRAWINGS FOR SURFACE AND HEIGHT OF UNITS, AND JOINT TYPE.
20. SAND SHALL BE CLEAN, SHARP AND WELL GRADED, FREE FROM INJURIOUS AMOUNTS OF DUST, HUMPS, SHALE, ALKALI OR ORGANIC MATERIAL.
21. ALL MORTAR FN OBSTRUCTIONS AND DEBRIS SHALL BE CLEANED FROM INSIDE OF CELLS PRIOR TO GROUTING.

NOTE:

- SEE CIVIL DRAWINGS FOR PAD LOCATIONS AND DIMENSIONS.
- SEE SHEETS GN-1 THROUGH GN-3 FOR GENERAL NOTES.
- SEE SHEET C-1 & C-2 FOR APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES. USE EXTREME CAUTION DURING EXCAVATION FOR NEW FOUNDATIONS.

FOUNDATION SCHEDULE			2000 PSF (ASSUMED)
CONTINUOUS FOOTINGS			
MARK	FOOTING SIZE WIDTH X THICKNESS X LENGTH	REINFORCING	
WF3.0	3'-0" X 1'-0" X CONT.	(3)#5 CONT.	(3)#5 TRANSVERSE @ 12" O.C.

FOUNDATION PLAN

CONCRETE MASONRY NOTES

C	NO SCALE
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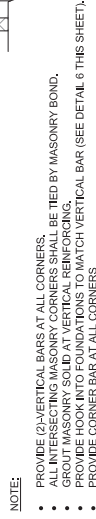
49030 Pontiac Trail, Suite 100
Wixom, Michigan 48393
PHONE: 248-705-9212

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	C	10/01/2022	CD100		
	B	06/11/2023	CD100		
	A	05/27/2022	CD950		
	REV	DATE	DESCRIPTION		



MASONRY ENCLOSURE DETAILS

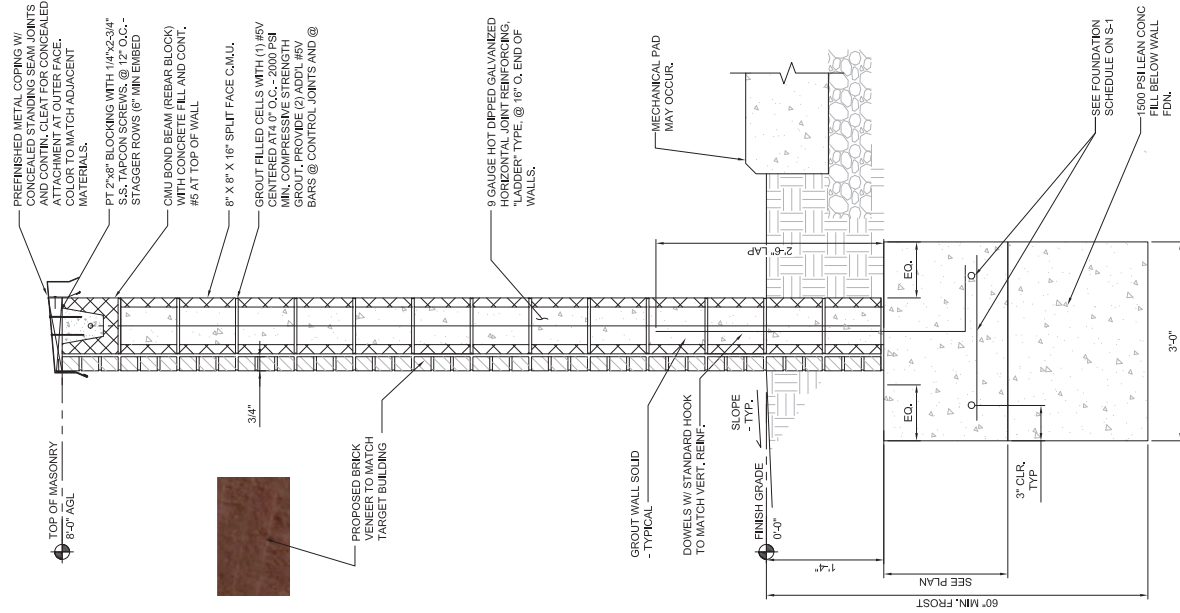
HEET NUMBER
S-2



DETAIL NOT USED



SCALE: 1 1/2" = 1'-0"

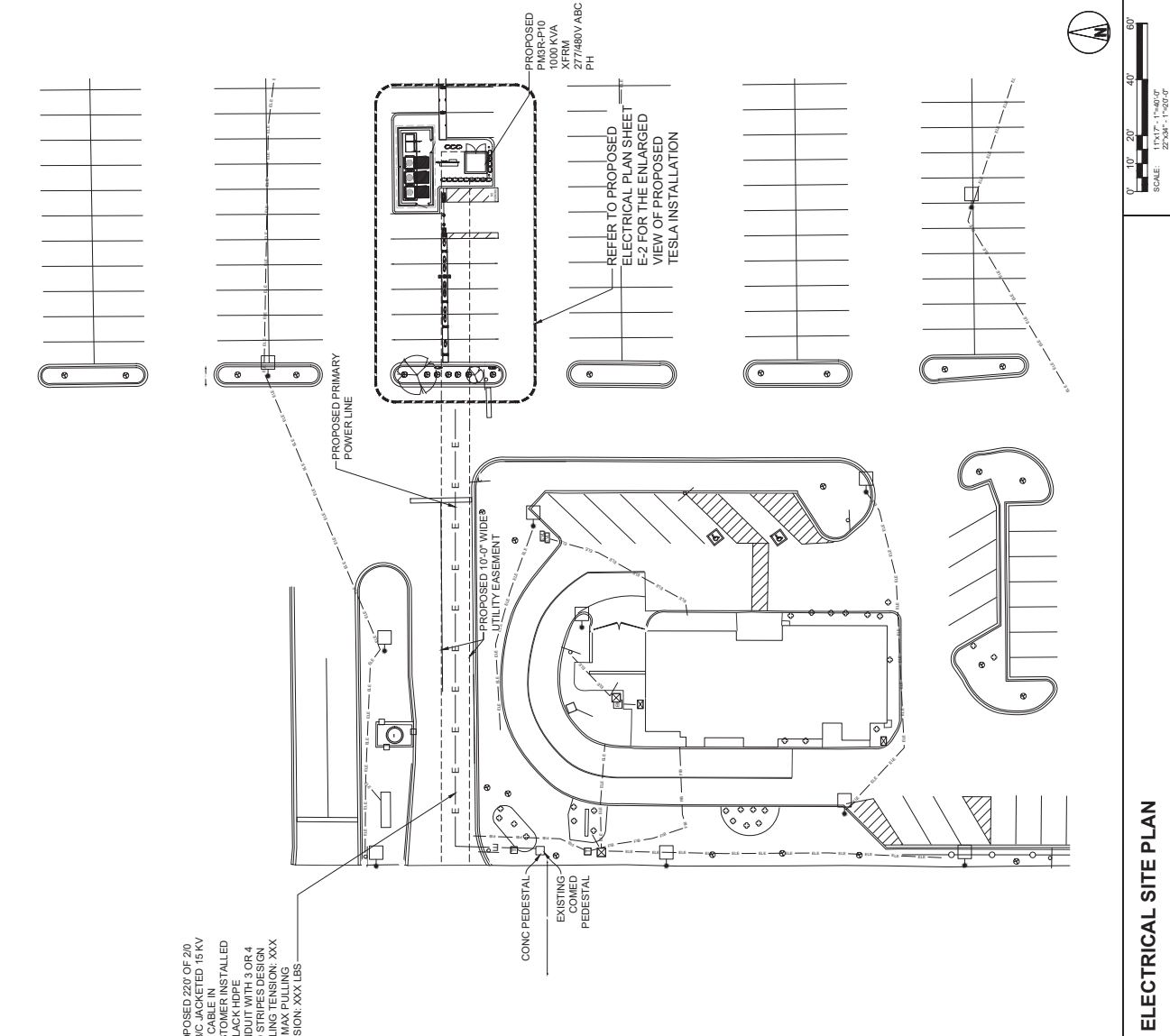


- NOTES:
1. THE UTILITY DESIGN DETAILS SUMMARIZED ON THIS SHEET ARE FOR CONSTRUCTION OF THE UTILITY FIELD INSPECTION PERSONNEL. THE UTILITY DESIGN DETAILS SUMMARIZED ON THIS SHEET ARE FOR CONSTRUCTION OF THE UTILITY FIELD INSPECTION PERSONNEL. THE UTILITY DESIGN DETAILS SUMMARIZED ON THIS SHEET ARE FOR CONSTRUCTION OF THE UTILITY FIELD INSPECTION PERSONNEL.
 2. UTILITY EQUIPMENT INSTALLATIONS AND PREP WORK AND TERMINATION OF SERVICE CONDUCTORS SHALL BE COORDINATED WITH THE APPROPRIATE UTILITY ENGINEER AT TIME OF PRECONSTRUCTION MEETING TO ENSURE ACCURACY OF INSTALLATIONS.
 3. TRANSFORMER BOLLARD PROTECTION TO BE INSTALLED PER UTILITY SPECIFICATION. ADDITIONAL BOLLARD PROTECTION MAY BE REQUIRED AT THE DISCRETION OF THE UTILITY FIELD INSPECTION PERSONNEL.


ELECTRICAL SCOPE OF WORK RESPONSIBILITIES		
SCOPE	BY UTILITY	BY TESLA
PROVIDE PRIMARY SIDE TRENCHING		X
PROVIDE & INSTALL PRIMARY SIDE CONDUITS		X
PROVIDE & INSTALL PRIMARY SIDE CONDUCTORS	X	
PROVIDE & INSTALL UTILITY TRANSFORMER PAD		X
PROVIDE UTILITY TRANSFORMER	X	
INSTALL UTILITY TRANSFORMER	X	
INSTALL CONNECTIONS AT UTILITY TRANSFORMER (PRIMARY)	X	
INSTALL CONNECTIONS AT UTILITY TRANSFORMER (SECONDARY)		X
PROVIDE METER BASE (UTILITY TO PROVIDER APPROVED SPECS)	X	
INSTALL METER BASE		X
PROVIDE METER	X	
INSTALL METER	X	
PROVIDE CTs	X	
INSTALL CTs (INSIDE CT CABINET)	X	
PROVIDE SECONDARY SIDE TRENCHING		X
PROVIDE & INSTALL SECONDARY SIDE CONDUITS W/ PULLWIRE		X
PROVIDE & INSTALL SECONDARY SIDE CONDUCTORS		X
PROVIDE ROAD CUTS / ROAD BORES / PAVEMENT REPLACEMENT		X
PROVIDE & INSTALL LANDSCAPE REMEDIATION		X

NOTE: SCOPE SHOWN ABOVE WAS PROVIDED BY NIPSCO. FIELD VERIFY PRIOR TO CONSTRUCTION.

NIPSCO CONTACT:
RODNEY GILSON
(219) 302-8743



ELECTRICAL SITE PLAN



3800 DIER CREEK RD
PALO ALTO, CA 94304
(650) 681-1000




48030 Pontiac Trail, Ste 400
Wyom., Michigan 48388
PHONE: 248-705-9212

DRAWN BY: JBR

CHECKED BY: RCH

REV	DATE	DESCRIPTION
E	07/11/2021	CD100
D	04/11/2021	CD100
C	10/11/2021	CD100
B	06/11/2021	CD100
A	02/27/2021	CD100

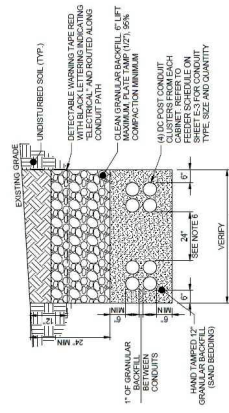


SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

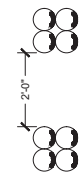
SHEET TITLE
ELECTRICAL SITE PLAN

SHEET NUMBER
E-1

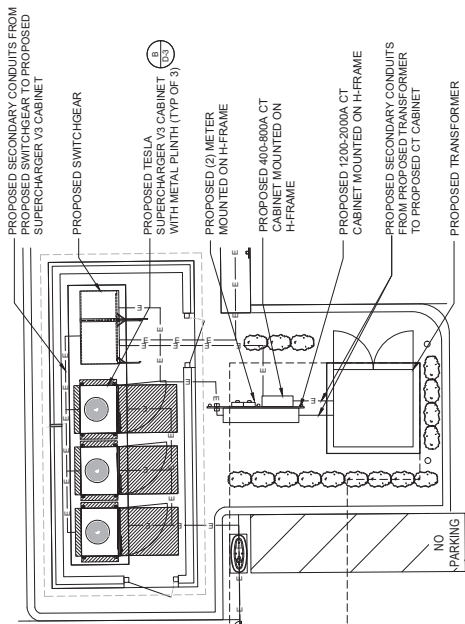
- NOTES:**
- BIDDING CONTRACTOR TO VERIFY DEPTHS AND LENGTHS IN FIELD.
 - THE EXACT ROUTING PATH AND CONDUCTOR RUN LENGTHS SHALL BE DETERMINED BY THE CONTRACTOR IN FIELD BASED ON PHYSICAL MEASUREMENTS. CONTRACTOR TO PROVIDE FIELD MEASUREMENTS TO FIELD MEASUREMENTS (MUST BE APPROVED BY TESLA INSTALLATION MANAGER).
 - ALL ELECTRICAL WORK AND RELATED ACTIVITIES PERFORMED ON-SITE SHALL BE DONE IN ACCORDANCE WITH NATIONAL ELECTRIC CODE (NEC) STANDARDS BEING ENFORCED BY ALL APPLICABLE JURISDICTIONAL REQUIREMENTS AT THE TIME OF CONSTRUCTION. THE MAXIMUM RUN LENGTH BETWEEN SUPERCHARGER CABINET AND CHARGING POST, INCLUDING BURIED DEPTH IS NOT TO EXCEED 330'.
 - SEE SHEET E-3 FOR CONDUIT AND WIRE SIZES.
 - UTILIZE SLURRY FOR ANY CONDUIT RUNS WHERE MORE THAN (4) CONDUITS ARE PRESENT.
 - ALL CONDUIT RUNS SHALL UTILIZE SCHEDULE 40 PVC OR HDPE.



- NOTES:**
- ANY EXCAVATION LEFT OPEN SHOULD BE SECURELY FENCED OFF.
 - CONTRACTOR SHALL INSTALL CONDUITS BELOW LOCAL FROST LINE. SHOULD FIELD CONDITIONS REQUIRE, CONTRACTOR TO RECONSTRUCT CONDITIONS FOR FILLERS.
 - VERIFY WIDTH OF TRENCH REQUIRED. REFER TO SITE ELECTRICAL DRAWING ON SHEET E-2 FOR DC TRENCH CONDUIT BANK DESIGN BY TESLA BASED ON RHD60 SOIL TYPE & BACKFILL.
 - CONTRACTOR CAN REDUCE SPACING AS REQUIRED BASED ON TESLA THERMAL MODELING CALCULATIONS. DC CONDUIT MAY BE ENCASED IN SLURRY TO DECREASE REQUIRED SPACING.



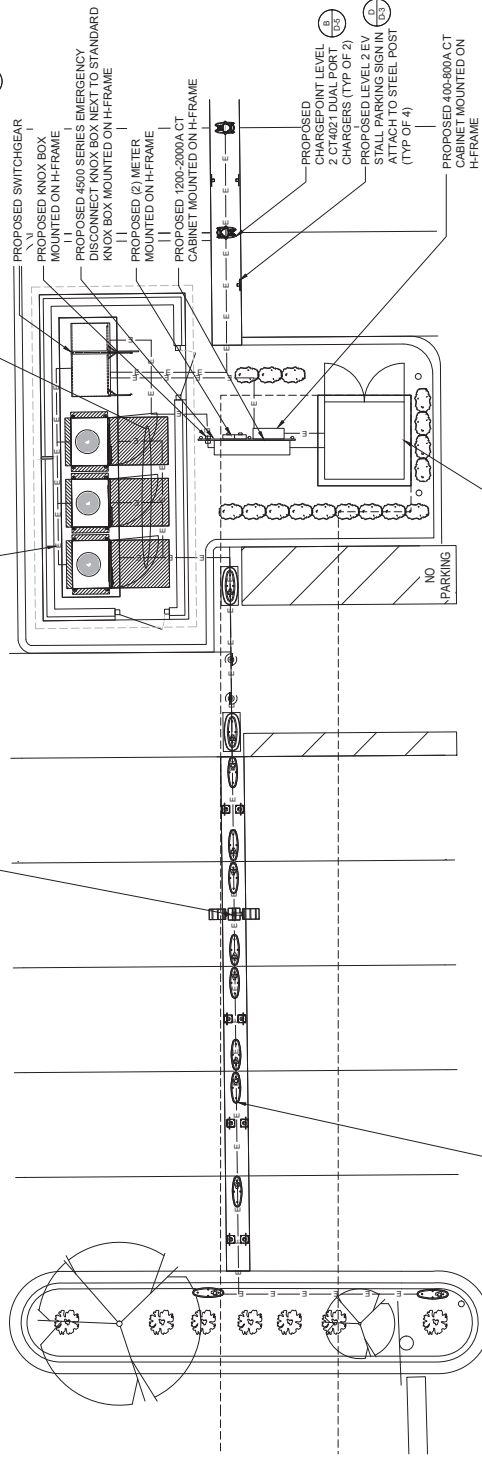
DC CHARGING POST
CONDUIT ARRANGEMENT DIAGRAM
NO SCALE



ENLARGED ELECTRICAL PLAN
SCALE: 11'x17" = 3/32" = 1'-0"
22'x34" = 3/16" = 1'-0"

PROPOSED TESLA SUPERCHARGER V3 CABINET WITH METAL PLINTH (TYP OF 3)

PROPOSED PEDESTRIAN DUAL LED LIGHT POLE AND FIXTURE



PROPOSED TESLA ILLUMINATED CHARGING STATION (TYP OF 12)

PROPOSED TRANSFORMER

REFERENCE SHEET E-1 FOR A SUMMARY OF THE UTILITY RELATED CONSTRUCTION RESPONSIBILITIES AND DESIGN DETAILS

CAUTION - ALL TRADES
USE EXTREME CAUTION IN AREA
OF EXISTING UTILITIES - HAND DIG -
USE ELECTRONIC DETECTION
PRIOR TO DIGGING

ELECTRICAL PLAN



3800 DIER, GREEK RD
PALO ALTO, CA 94304
(415) 667-0000



48030 Pontiac Trail, Ste 400
Wyom., Michigan 48388
PHONE: 248-705-9212

DRAWN BY: JBR
CHECKED BY: RCH

REV	DATE	DESCRIPTION
E	07/11/2023	CD100
D	04/11/2023	CD100
C	10/11/2022	CD100
B	06/11/2022	CD100
A	09/27/2022	CD100



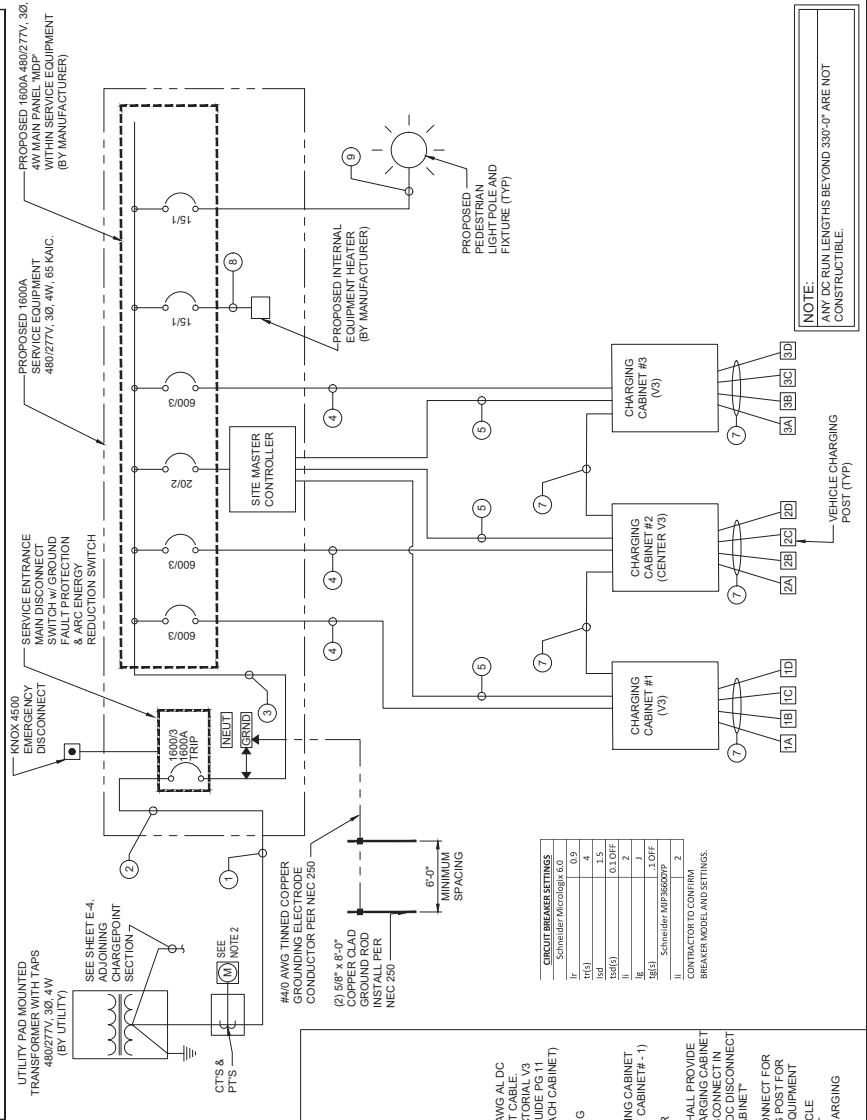
SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

SHEET TITLE
**ELECTRICAL PLANS &
LOAD SCHEDULE**

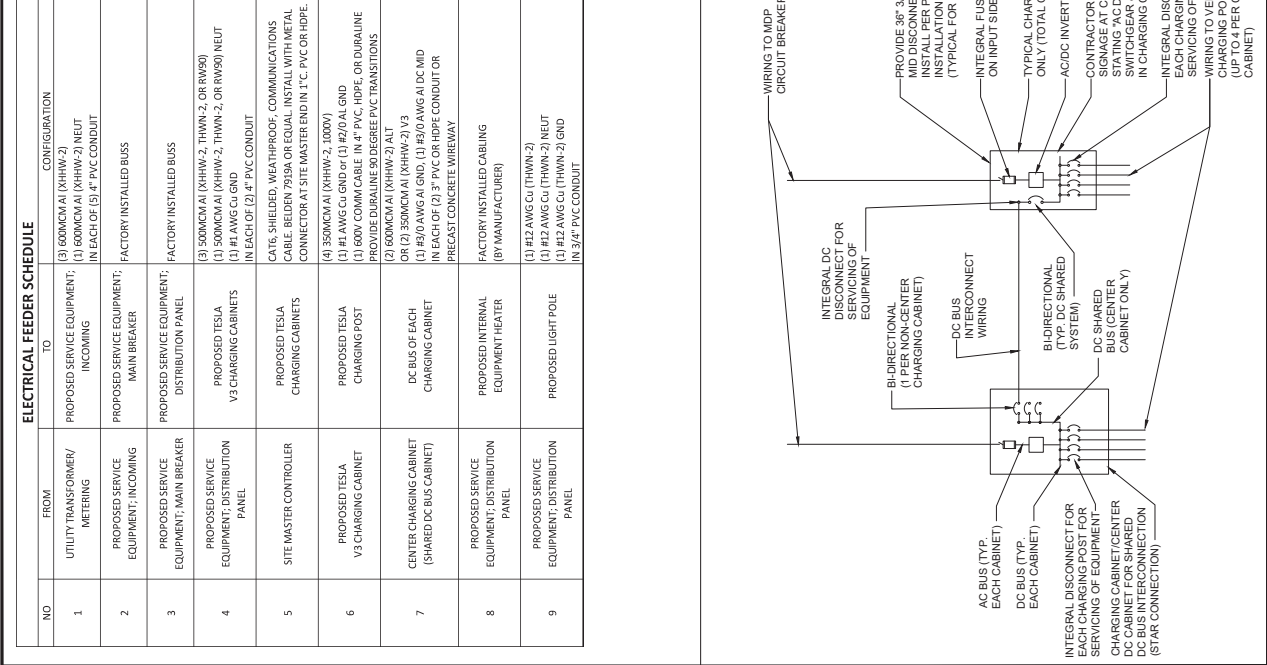
SHEET NUMBER
E-2

ELECTRICAL FEEDER SCHEDULE		
NO	FROM	TO
1	UTILITY TRANSFORMER/ METERING	(3) 600MCM AL (XHHW-2) (1) 600MCM AL (XHHW-2) NEUT IN EACH OF (3) 4" PVC CONDUIT
2	PROPOSED SERVICE EQUIPMENT; INCOMING	FACTORY INSTALLED BUS
3	PROPOSED SERVICE EQUIPMENT; MAIN BREAKER	FACTORY INSTALLED BUS
4	PROPOSED SERVICE EQUIPMENT; DISTRIBUTION PANEL	(3) 500MCM AL (XHHW-2, THWN-2, OR RW90) (1) 500MCM AL (XHHW-2, THWN-2, OR RW90) NEUT IN EACH OF (3) 4" PVC CONDUIT
5	SITE MASTER CONTROLLER	CAT5, SHIELDED, WEATHERPROOF, COMMUNICATIONS CABLE, BULKHEAD 7/16" OR EQUAL, INSTALL WITH METAL CONNECTOR AT SITE MASTER END IN 1" PVC OR HDPE.
6	PROPOSED TESLA V3 CHARGING CABINET	(4) 350MCM AL (XHHW-2, 1000V) (1) #1 AWG CU GND OR (1) #2/0 AL GND (1) 600V COMM CABLE IN 4" PVC, HDPE, OR DURALINE PROVIDE DURALINE 90 DEGREE PVC TRANSITIONS (2) 600MCM AL (XHHW-2) ALT OR (2) 350MCM AL (XHHW-2) V3 (1) #3/0 AWG AL GND, (1) #3/0 AWG AL DC MID IN EACH OF (2) 3" PVC OR HDPE CONDUIT OR PRECAST CONCRETE WIREWAY
7	CENTER CHARGING CABINET (SHARED DC BUS CABINET)	FACTORY INSTALLED CABLING (BY MANUFACTURER)
8	PROPOSED SERVICE EQUIPMENT; DISTRIBUTION PANEL	FACTORY INSTALLED CABLING (BY MANUFACTURER)
9	PROPOSED SERVICE EQUIPMENT; DISTRIBUTION PANEL	(1) #12 AWG CU (THWN-2) (1) #12 AWG CU (THWN-2) NEUT (1) #12 AWG CU (THWN-2) GND IN 3/4" PVC CONDUIT

- GENERAL SHEET NOTES
- NEUTRAL MUST BE INCLUDED FOR PROPER OPERATION OF TESLA SUPERCHARGERS.
 - PROPOSED UTILITY PTS & CTS SHALL BE LOCATED IN H-STAND MOUNTED CT CABINET. PROPOSED METER SHALL BE MOUNTED ON H-STAND. COORDINATE EXACT WIRING WITH UTILITY. PROVIDE 1" TO METER.
 - SEE SHEET E-2 FOR PANEL SCHEDULES.
 - ALL CONDUIT FURNISHED AND INSTALLED BY CONTRACTOR. ALL WIRING FURNISHED BY TESLA AND INSTALLED BY CONTRACTOR.
 - ALL CONDUITS ACCESSIBLE TO THE GENERAL PUBLIC OR WHICH CONDUITS CAN BE DAMAGED SHALL BE RIGID GALVANIZED STEEL.
 - ALL BUSBARS AND INTERNAL WIRING OF PROPOSED SERVICE EQUIPMENT PROVIDED BY MANUFACTURER. ANY MODIFICATIONS SHALL REQUIRE ENGINEERING APPROVAL PRIOR TO ANY CHANGES BEING MADE.
 - CONTRACTOR SHALL PERFORM ARC FLASH CALCULATIONS AS REQUIRED IN THE FOLLOWING: NFPA 70; NFPA 70E; OSHA 29; AND IEEE STANDARDS 1584. CONTRACTOR SHALL OBTAIN ALL NECESSARY INFORMATION FROM POWER COMPANY TO CALCULATE FLASH PROTECTIVE LEVELS. CONTRACTOR SHALL PROVIDE ALL NECESSARY LABELS AND WARNING LABELS FOR COMPLETING THE WARNING LABELS. PROVIDE WARNING LABELS CONTAINING ALL THE LATEST INFORMATION AS REQUIRED BY LOCAL JURISDICTION, STATE AND FEDERAL CODES AND LAWS.
 - CONTRACTOR SHALL VERIFY AC AND DC WIRING REQUIREMENTS WITH VENDORS DIRECTLY WITH TESLA.
 - VERIFY AVAILABLE FAULT CURRENT AT THE SECONDARY OF THE UTILITY TRANSFORMER WITH THE POWER COMPANY. CONDUCT A FAULT CURRENT ANALYSIS TO DETERMINE THE INTERRUPTING CAPACITY (AIC RATING) OF THE ELECTRICAL EQUIPMENT. AIC RATING OF EQUIPMENT SHALL BE BASED UPON CONTRACTOR'S FAULT CURRENT ANALYSIS.
 - ALL ALUMINUM (AL) CONDUCTORS TO RECEIVE ANTI-OXIDATIVE COATING DURING INSTALLATION. ALL OTHER CONDUCTORS ARE COPPER UNLESS NOTED OTHERWISE.
 - THE CHARGING CABINETS AND THE CHARGING POSTS USED ON THIS PROJECT COMPLY WITH THE FOLLOWING STANDARDS:
 - UL 2202
 - UL 2202, NO 107.1-16
 - UL 1988 PENDING
 - THE AFOREMENTIONED STANDARDS IDENTIFY THE REQUIREMENTS MET BY THE EQUIPMENT, INCLUDING BUT NOT LIMITED TO:
 - PROTECTION AGAINST ELECTRIC SHOCK
 - PROTECTION AGAINST SHORT CIRCUIT PROTECTION
 - FAULT PROTECTION
 - DEGREES OF PROTECTION AGAINST ACCESS TO HAZARDOUS LIVE PARTS
 - THE INTERNAL COMPONENTS OF THE SYSTEM ARE PROPRIETARY. ANY QUESTIONS CONCERNING ACTUAL INTERNAL PROTECTIVE DEVICES MUST BE COORDINATED DIRECTLY WITH TESLA.
 - CONTRACTOR SHALL VERIFY AC AND DC WIRING REQUIREMENTS WITH VENDORS DIRECTLY WITH TESLA.



TYPICAL SYSTEM ONE-LINE DIAGRAM





3500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 681-5000

49030 Pontiac Trail, Ste 400
Wixom, Michigan 48393
PHONE: 248-705-9212

DRAWN BY:	JSR
CHECKED BY:	RCH

REV	DATE	DESCRIPTION
A	05/27/2023	CD90
B	06/11/2023	CD100
C	10/01/2022	CD100
D	04/11/2023	CD100
E	07/31/2023	CD100

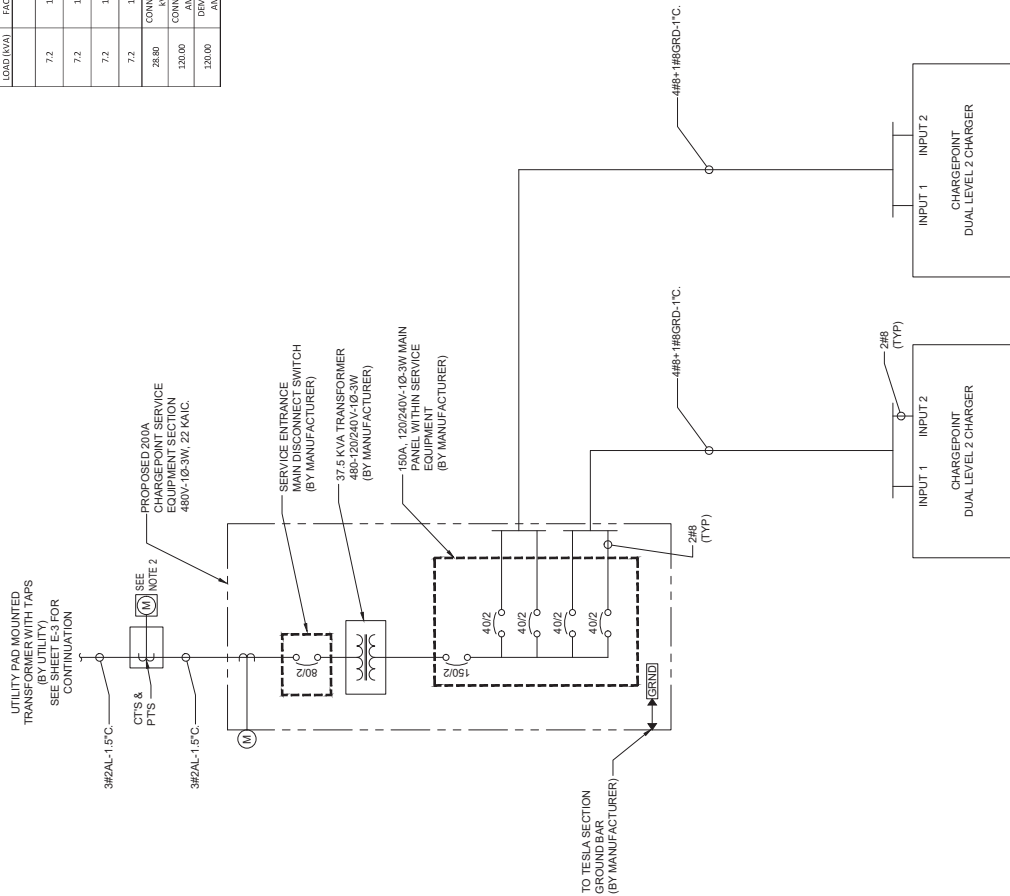


SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

CHARGEPOINT SYSTEM

F-4
SHEET NUMBER

SITE ID NUMBER IN CHARGEPOINT	MODEL # LINCOLN	WIRE #
VOLTAGE 120/240V	BUS RATING: 200 AMP	GND BAR: YES
PHASE 3P	NEU BAR: YES	IN TO GROUND: YES SEE A/E-3
SERVICE USAGE FACTOR	BREAKER STATUS	LOAD DESCRIPTION
7.2	ON	MAIN BREAKER
7.2	ON	CHARGEPOINT C1402 P1
7.2	ON	CHARGEPOINT C1402 B1
7.2	ON	CHARGEPOINT C1402 P2
7.2	ON	CHARGEPOINT C1402 B2
CONNECTED	KVA	
28.80	CONNECTED	
120.00	AMPS	
120.00	UNPAID AMPS	



NOTE:
REFER TO NOTES ON SHEET F-3

CHARGEPOINT SYSTEM ONE-LINE DIAGRAM

NO SCALE

A



49030 Pontiac Trail, Suite 100
Wixom, Michigan 48393
PHONE: 248-705-9212

DRAWN BY:	RC
CHECKED BY:	PL

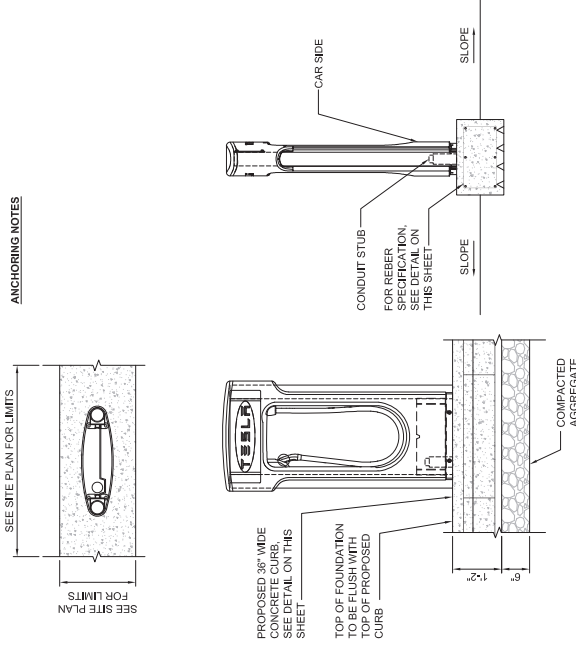
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C	10/01/2022	CD100
D	04/11/2023	CD100
E	07/31/2023	CD100



SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

INSTALLATION DETAILS

SHEET NUMBER
D-1



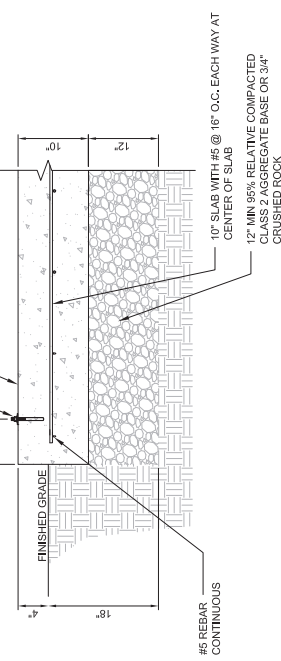
NOTE: MAX. CONTINUOUS SLAB LENGTH SHOULD NOT EXCEED 50 FT WHO EXPOSITION JOINT

1/2" DIAMETER SS THREADED ROD, W/ HILTI HIT-HY 200 ADHESIVE ANCHOR SYSTEM WITH 3'-14" MIN. EMBEDMENT INTO (N) SLAB IN ACCORDANCE WITH ESR-3187 OR APPROVED EQUAL. DRILL AND SET OF EPOXY ANCHORS SHALL NOT OCCUR LESS THAN 7 DAYS FROM POURING CONCRETE.

IN ORDER TO MAINTAIN POSITIVE DRAINAGE, THE SLAB SHALL SLOPE 0.5% TOWARDS THE DOWNHILL DIRECTION

6" MIN

EDGE DISTANCE



CHARGING POST CONTINUOUS CURB FOUNDATION

NO SCALE

CHARGING POST CONTINUOUS CURB FOUNDATION

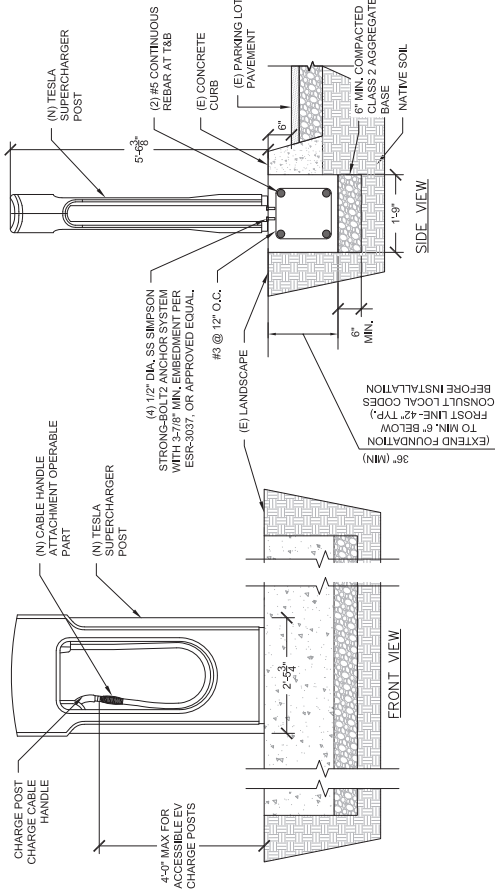
A

NO SCAL

EQUIPMENT PAD & ANCHOR SECTION

NOTES	SEE
1.	CH
2.	SH
3.	RE
4.	ALL CO
5.	ALL SH ST

**NOTE: CAST IN PLACE
FOUNDATION FOOTINGS MUST
EXTEND 6" BELOW FROST LINE-
CONSULT WITH LOCAL CODE
BEFORE INSTALLATION**

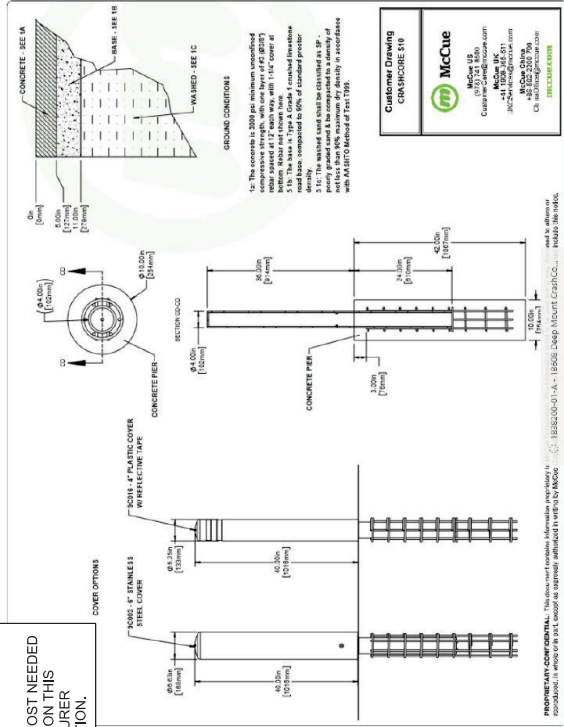


CHARGING POST DETAIL FOR 1A STALL

CHARGING POST CONTINUOUS CURB REINFORCEMENT DETAIL

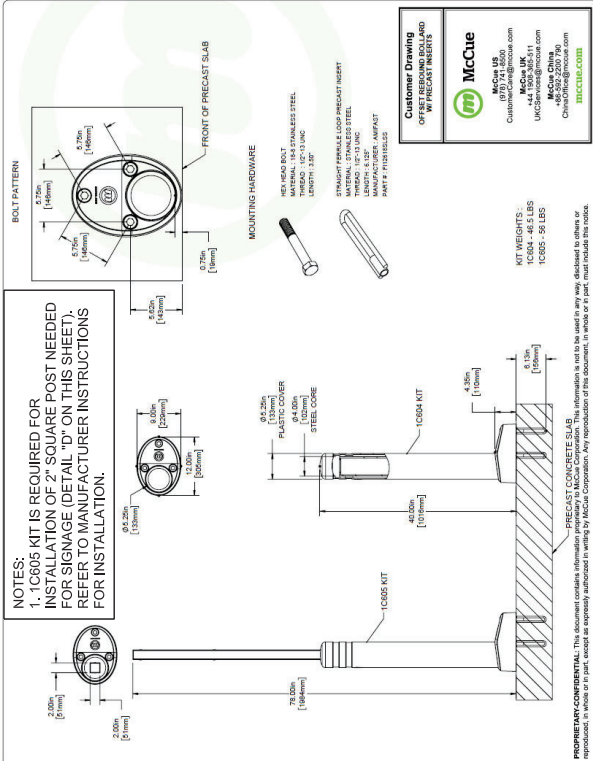
D

NOTES:
1. 1C605 KIT IS REQUIRED FOR
INSTALLATION OF 2" SQUARE POST NEEDED
FOR ADA SIGNAGE (DETAIL "D" ON THIS
SHEET). REFER TO MANUFACTURER
INSTRUCTIONS FOR INSTALLATION.



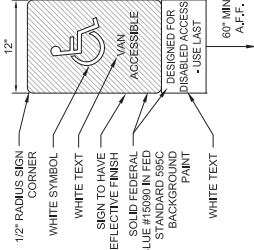
BOLLARD WITH SIGN INSTALLATION MANUFACTURER DETAIL

NOTES:
1. 1C605 KIT IS REQUIRED FOR
INSTALLATION OF 2" SQUARE POST NEEDED
FOR SIGNAGE (DETAIL "D" ON THIS SHEET).
REFER TO MANUFACTURER INSTRUCTIONS
FOR INSTALLATION.

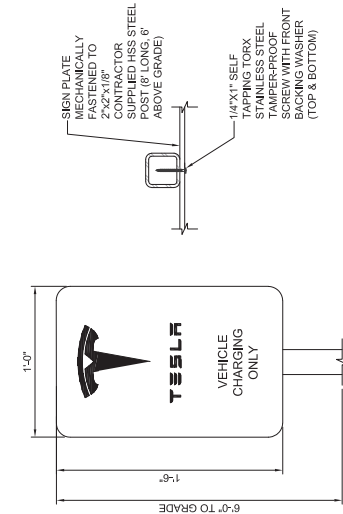


BOLLARD WITH SIGN INSTALLATION DETAIL

MOUNTING OPTIONS:
SIGN CAN BE POLE MOUNTED PER SPECIFICATIONS IN
POLE MOUNTED SIGN DETAILS.
(WITH TAMPER RESISTANT SORENS)
SIGN MATERIAL: ALUMINUM



TESLA DEDICATED ADA STALL PARKING SIGN DETAIL



NOTES

1. SIGN AND TAMPER PROOF HARDWARE SCREWS TO BE INSTALLED.
2. POST SHALL BE PROVIDED BY CONTRACTOR.
3. IF PAINT FINISH IS DAMAGED DURING INSTALLATION, CONTRACTOR SHALL REPAINT AS REQUIRED.
4. NON-ILLUMINATED POLE MOUNT PARKING SIGN FACES AND RETURNS TO BE 0.090\"/>
5. LOGO TO BE #280-10 REFLECTIVE WHITE VINYL (VERIFY REFLECTIVITY WITH OWNER).
6. SEE TYPICAL TESLA PARKING SIGNAGE DETAIL FOR SIGNAGE VARIATION (IF APPLICABLE).
7. SEE TYPICAL TESLA PARKING SIGNAGE DETAIL FOR SIGNAGE VARIATION (IF APPLICABLE).
8. SIGN POST SHALL BE POWDER COATED, COLOR - MEDIUM/DARK GREY
9. SIGN POST SHALL BE PLUGGED/CAPPED TO PREVENT WATER INTRUSION FROM TOP.
10. SIGN POST SHALL BE MOUNTED TO LIGHT POLE WHERE APPLICABLE, SEE PLAN.

TESLA DEDICATED NON-ILLUMINATED PARKING SIGN DETAIL



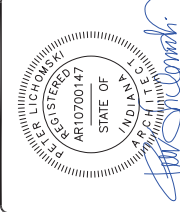
3500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 697-5000

LAB

49030 Pontiac Trail, Suite 100
Milcom, Michigan 48363
PHONE: 248-705-9212

DRAWN BY:	RC
CHECKED BY:	PL

REV	DATE	DESCRIPTION
E	07/21/2023	CD100
D	04/11/2023	CD100
C	10/01/2022	CD100
B	06/11/2022	CD100
A	05/27/2022	CD09



SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

INSTALLATION DETAILS

SHEET NUMBER
D-2

3500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 681-5000



49030 Pontiac Trail, Suite 100
Wixom, Michigan 48393
PHONE: 248-705-9212

DRAWN BY:	RC
CHECKED BY:	DI

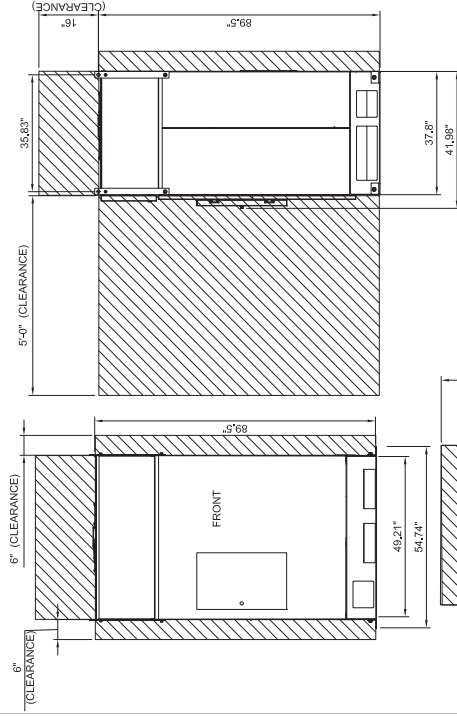
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A	05/27/2022	CD500



SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

INSTALLATION DETAILS

D-3

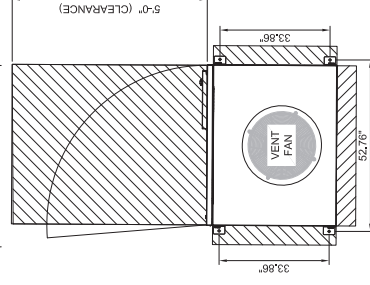


TESLA CHARGING CABINET

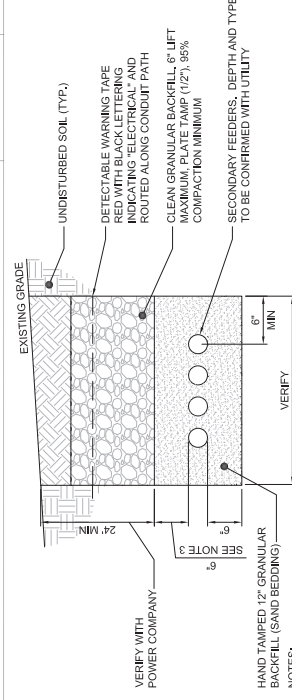
ENCLOSURE: INGRESS PROTECTION IP66
WEIGHT: 1500 KG, 3307 LBS.
COMPLIANCE: UL 2202, CSA 22.2 NO 107.1-
UL1998 PENDING

NOTES:

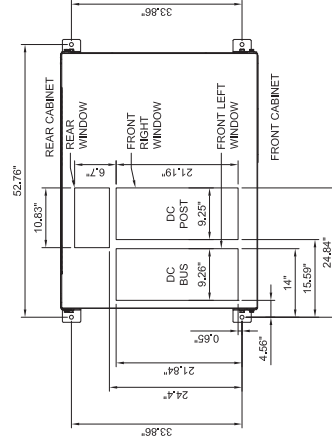
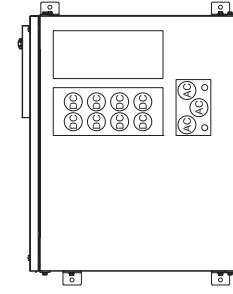
1. CABINET SHOULD BE LIFTED USING ROOF MOUNTED EYE HOOKS. A FORKLIFT OR PALLET JACK CAN ALSO BE USED TO MOVE CABINET IF DONE PROPERLY.
2. SEE GN-3 FOR CHARGING CABINET NOTE.



TYPICAL TESLA SUPERCHARGER V3 CABINET
MANUFACTURER DETAIL S - FOR REFERENCE ONLY



- NOTES:
1. ANY EXCAVATION LEFT OPEN SHOULD BE SECURED AND COVERED.
 2. ANY PAVEMENT DAMAGE DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR TO PRECONSTRUCTION CONDITIONS OR BETTER.
 3. CONTRACTOR SHALL INSTALL CONDITIONS BELOW LOCAL FROST LINE. SHOULD FIELD CONDITIONS VARY, CONTRACTOR SHALL COORDINATE WITH CONTACT ENGINEER. LISTED ON SHEET T-1.
 4. WIDTH OF TRENCH REQUIRED, REFER TO SITE ELECTRICAL DRAWING ON SHEET E-2 FOR ROUTING.
 5. VERIFY ALL REQUIREMENTS WITH POWER COMPANY.

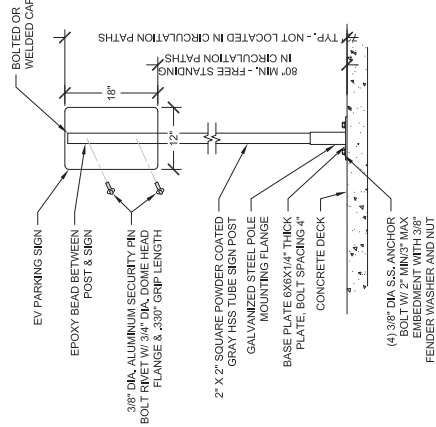


CONDUIT AND CONDUCTOR POSITIONING	
REAR WINDOW	AC CONDUITS
FRONT WINDOW	DC BUS CONDUITS
FRONT RIGHT WINDOW	DC POST CONDUITS
	ETHERNET CABLE FOR TESLA SITE CONTROLLER
	24V DC OPTIONAL BACKUP POWER FOR TESLA SITE CONTROLLER

CONDUIT POSITIONING AND ANCHORING LOCATIONS

CONDUIT POSITIONING AND ANCHORING LOCATIONS

- NOTES:
1. SIGNAGE MUST BE PLACED WITHIN THE PROJECTED HEAD END OF THE PARKING SPACE
2. VERIFY LOCATION OF POST TENSION CABLES AND REINFORCEMENT AT EXISTING CONCRETE DECK PRIOR TO INSTALLATION OF ANCHOR BOLTS.



	C	D
NOT USED	NO SCALE	NO SCALE
		POST MOUNTED SIGN - SURFACE MOUNTED CHARGEPOINT SIGN DETAIL

<div> <div> <p>BRILLIANT LIGHTING & SPECIALTY PRODUCTS</p> <p>BIPSS Straight Steel Pole, 4" - 8"</p> </div> <div> <p>Pole Series</p> <p>The BIPSS is a 100% American-made, heavy-duty steel pole with a 10-year warranty. It is designed for use in a variety of applications, including street lighting, security lighting, and general outdoor lighting. The pole is made of high-quality steel and is available in a variety of heights and configurations.</p> <p>Specifications and Features:</p> <ul style="list-style-type: none"> 100% American-made Heavy-duty steel construction 10-year warranty Available in a variety of heights and configurations Designed for use in a variety of applications <p>Order Information:</p> <p>Product Name: BIPSS Product Code: BIPSS-4-8 Product Description: Straight Steel Pole, 4" - 8"</p> </div> </div>	<div> <div> <p>Linmore LED LED PERFORMANCE LIGHTING</p> </div> <div> <p>SITE LIGHTER (SLI)</p> <p>Linmore LED's Site Lighter (SLI) is a superior outdoor lighting solution for your site. It is a compact, energy-efficient, and durable lighting fixture that is designed to provide uniform, high-quality illumination for your site. The SLI is made of high-quality aluminum and is available in a variety of sizes and configurations.</p> <p>Specifications and Features:</p> <ul style="list-style-type: none"> Compact and energy-efficient Durable aluminum construction Available in a variety of sizes and configurations Designed for use in a variety of applications <p>Order Information:</p> <p>Product Name: Site Lighter (SLI) Product Code: SLI-1000 Product Description: Compact, energy-efficient outdoor lighting fixture</p> </div> </div>	<div> <div> <p>TESLA</p> <p>3500 DEER CREEK RD PALO ALTO, CA 94304 (650) 354-0000</p> </div> <div> <p>LAB</p> <p>49030 Pontiac Trail, Suite 100 Wktn, Michigan 48393 PHONE: 248-705-9212</p> </div> <div> <p>DRAWN BY: RC CHECKED BY: PL</p> </div> <div> <p>REVISIONS:</p> <table border="1"> <thead> <tr> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td>E</td> <td>07/19/2023</td> <td>CD100</td> </tr> <tr> <td>D</td> <td>04/11/2023</td> <td>CD100</td> </tr> <tr> <td>C</td> <td>10/01/2022</td> <td>CD100</td> </tr> <tr> <td>B</td> <td>06/11/2022</td> <td>CD100</td> </tr> <tr> <td>A</td> <td>05/27/2022</td> <td>CD100</td> </tr> </tbody> </table> </div> <div> </div> <div> <p>SITE NAME: MUNSTER, IN 8005 CALUMET AVE MUNSTER, IN 46321</p> </div> <div> <p>INSTALLATION DETAILS</p> </div> <div> <p>SHEET NUMBER D-4</p> </div> </div>	REV	DATE	DESCRIPTION	E	07/19/2023	CD100	D	04/11/2023	CD100	C	10/01/2022	CD100	B	06/11/2022	CD100	A	05/27/2022	CD100	<div> <div> <p>TYPICAL PEDESTRIAN LIGHT FIXTURE MANUFACTURER DETAIL - FOR REFERENCE ONLY</p> </div> <div> <p>NO SCALE</p> </div> <div> <p>PEDESTRIAN LIGHT POLE MANUFACTURER DETAIL - FOR REFERENCE ONLY</p> </div> <div> <p>NO SCALE</p> </div> <div> <p>NOT USED</p> </div> </div>
REV	DATE	DESCRIPTION																			
E	07/19/2023	CD100																			
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C	10/01/2022	CD100																			
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Concrete Mount Kit

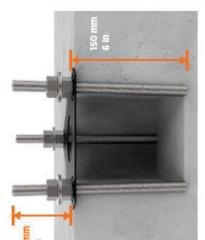
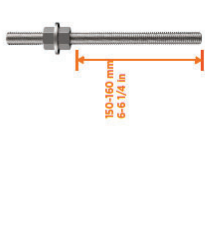
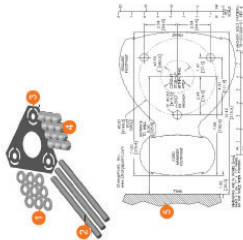
ChargePoint offers an optional CT4000 Concrete Mount Kit for purchase. The kit contains all parts needed to install the CT4000 pedestal mount into new or existing concrete.

- Kit Contents
- 9 galvanized washers
 - 3 hot-dipped galvanized threaded bolts
 - 1 plastic bolt installation template
 - 12 hex nuts
 - CT4000 installation template with CMK footprint

Note: The Concrete Mount Kit contains 12 hex nuts and 9 galvanized washers. You need only 6 of each for installation on existing concrete pad.

Installation Instructions

- Install two nuts, with two washers captured between them. Lock them together so the lower end of the upper nut is located 150 to 160 mm (6 to 6 1/4 in) from the bottom of the bolt. This sets the length of the exposed threads.
- Place the plastic bolt installation template to mark the hole locations.



- Remove the template and drill three 25 mm (1 in) diameter holes 150 mm (6 in) deep into the concrete.
 - When locating the template, consider the charging station's total footprint.
 - It is important that the bolts are parallel after installation. Ensure the drill holes are plumb by using a level to check the angle of the drill after drilling 25 to 38 mm (1 to 1 1/2 in).
 - If installing over existing buried conduit, position the center of the template around the conduit stub-up.

- You may need two drill bits: one for the concrete (with the pilot) and another for the rebar (without the pilot). Always start the hole using the standard drill bit, then switch to the rebar drill bit only if drilling through rebar.

- Remove all dust from inside the drilled holes using compressed air, a vacuum, and/or a brush.

- If the concrete slab is only 150 mm (6 in) deep, insert a plug (such as McMaster product #975K456) in each hole to keep the epoxy in place until it hardens. Place the plug over the long end of a bolt and then use the bolt to push the plug to the bottom of the hole.

- Fill each hole with epoxy to about 65 to 75 mm (2 1/2 to 3 in) below the top. Continue immediately to the next step because the epoxy sets quickly.

Note: Inserting the threaded bolts displaces the epoxy, causing it to fill the holes to the grade level. If the epoxy is below grade level after the next step, add more epoxy.

- Place the plastic concrete bolt installation template over the holes. This ensures the relative position of the bolts and that the flange of the pole fits over the bolts.

- Insert the bolts through the template, into the holes.

Important: Rotate the bolts as you insert them. This allows the epoxy to fully coat the threads of the bolts, reducing the amount of trapped air.

Note: The installation template can be left in place.

- If needed, top the holes with epoxy to grade level.

- Use a bubble level to ensure the bolts are plumb.

- Allow the epoxy to cure (depending on cure times recommended by the epoxy manufacturer) before removing the top nuts and washers.

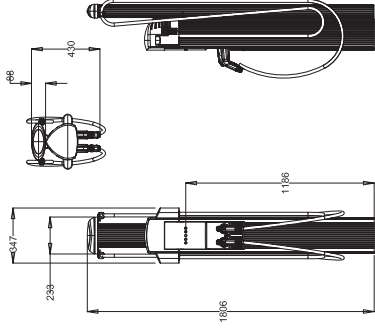
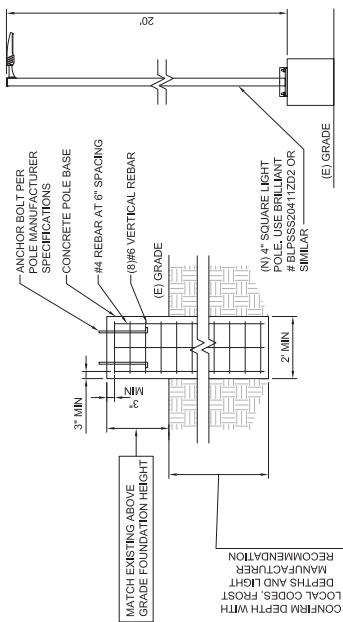
- Allow the epoxy to fully cure (depending on cure times recommended by the epoxy manufacturer) before applying torque to the nuts.



You are now ready to install the CT4000 pedestal mount charging station.

NOTE: CAST IN PLACE CONCRETE FOUNDATION MUST EXTEND 6" BELOW FROST LINE-CONSULT WITH LOCAL CODE BEFORE INSTALLATION

NOTE: FOUNDATION REVIEW ABOVE MUST BE MATCHED TO CHARGING STATION BUT NOT TO EXCEED 3".



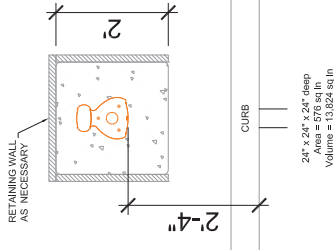
LIGHTPOST FOUNDATION DETAIL

NO SCALE

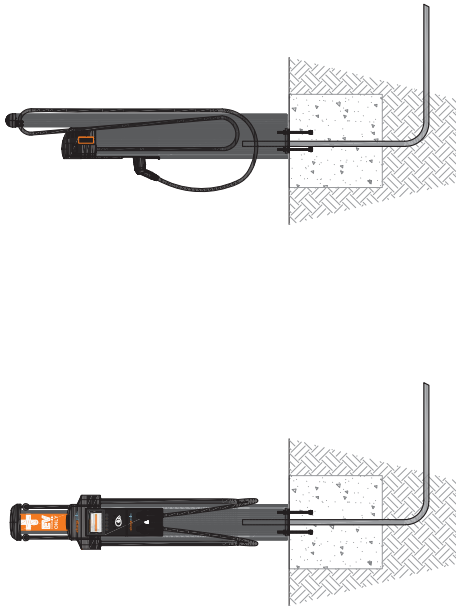
CHARGEPOINT LEVEL 2 CT4021 DUAL PORT CHARGERS DETAIL

NO SCALE

B



IN PLANTER OR BERM BETWEEN SPACES



CHARGEPOINT FOUNDATION INSTALLATION SPECS

NO SCALE

TYPICAL CHARGEPOINT FOUNDATION DETAIL

NO SCALE

D



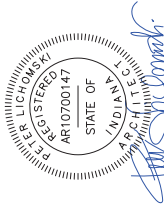
3500 DEER CREEK RD
PALO ALTO, CA 94304
(650) 997-5000



49030 Pontiac Trail, Suite 100
Livonia, Michigan 48333
PHONE: 248-705-9212

DRAWN BY:	RC
CHECKED BY:	PL

E	07/10/2022	CD100
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REV	DATE	DESCRIPTION



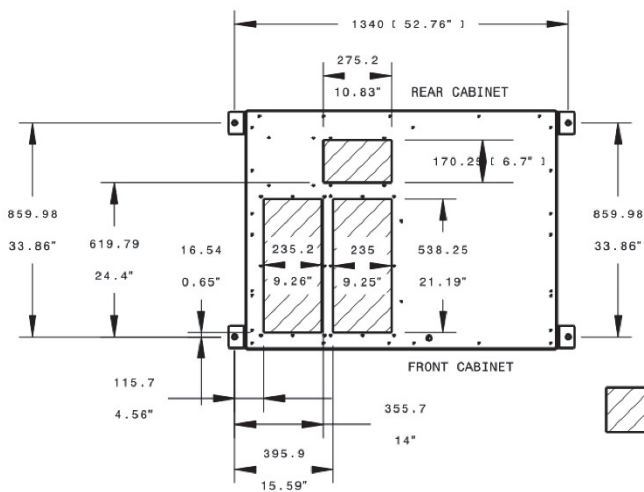
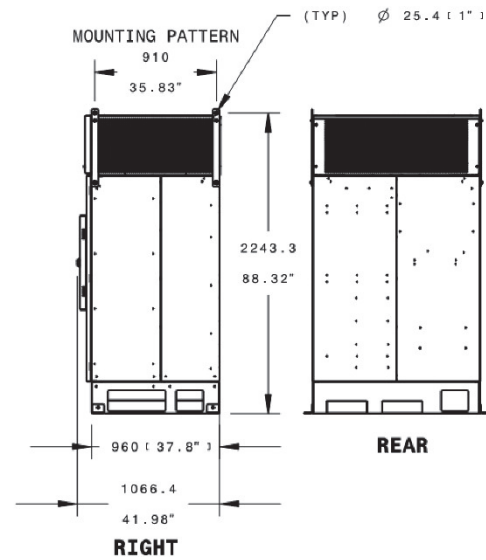
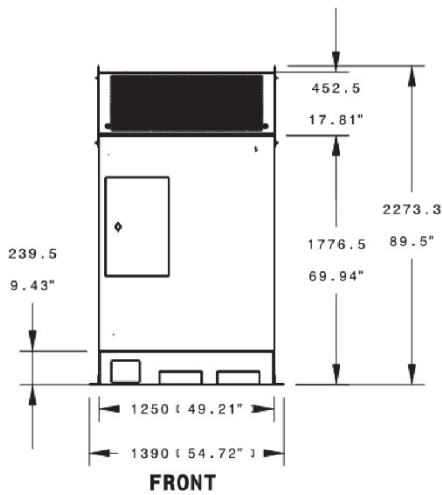
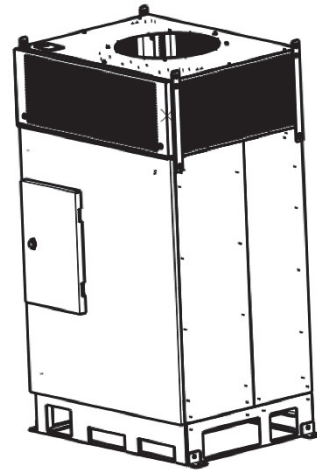
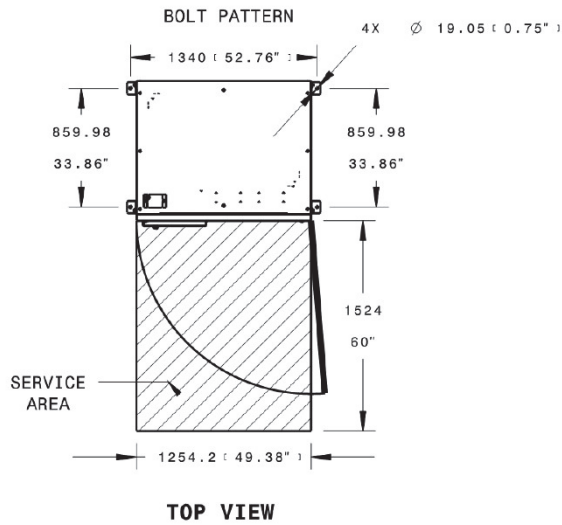
SITE NAME: MUNSTER, IN
8005 CALUMET AVE
MUNSTER, IN 46321

INSTALLATION DETAILS

SHEET TITLE

SHEET NUMBER

D-5

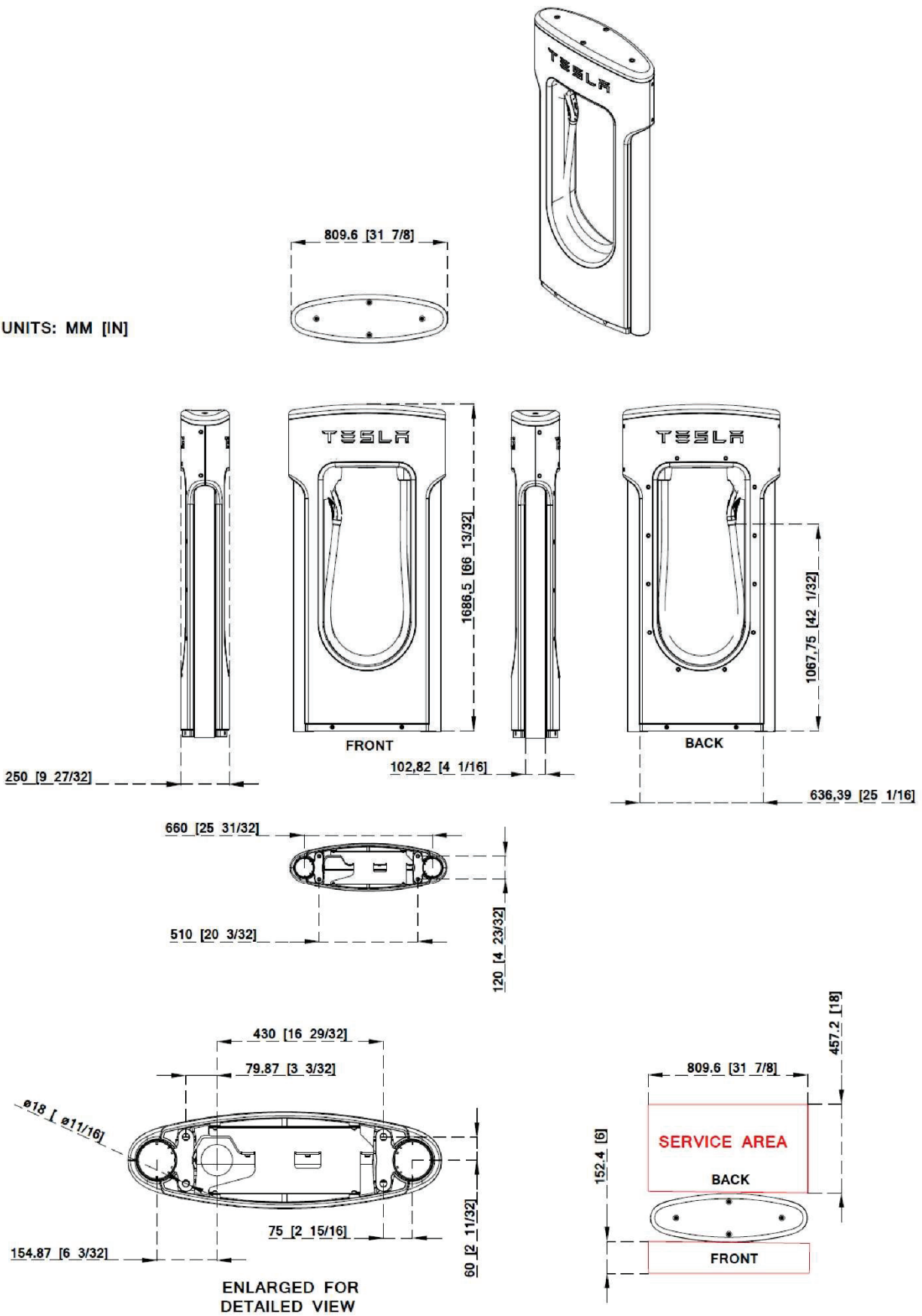


INSIDE CABINET
ENLARGED FOR
DETAILED VIEW

Tesla Site Controller



UNITS: MM [IN]



The MUNSTER BOARD OF ZONING APPEALS
MINUTES OF SPECIAL BUSINESS MEETING
Meeting Date: November 15, 2022

The announced meeting location was Munster Town Hall and could be accessed remotely via Zoom, a video conference application.

Call to Order: 6:45 pm

Pledge of Allegiance

Members in Attendance:

Sharon Mayer
Roland Raffin
Brad Hemingway

Members Absent:

Daniel Buksa
Brian Specht

Staff Present:

Tom Vander Woude, Planning Director
Dave Wickland, Attorney

Approval of Minutes:

Motion: Mr. Raffin moved to approve the minutes of the October 11, 2022, regular meeting.

Second: Mr. Hemingway

Vote: Yes – 3 No – 0 Abstain – 0. Motion carries

Preliminary Hearings: None

Public Hearings:

Additional Business/Items for Discussion:

- a. Request for determination whether electric vehicle charging stations are a permitted accessory in the CD-4.A, CD-4.B, CD-5, CZ, SD-PUD, and SD-M districts.**

Mr. Vander Woude said the zoning ordinance provides that ...If the Zoning Administrator is in doubt as to the meaning or intent of any provision of this Article... such person shall refer the matter to the Board of Zoning Appeals for interpretation and decision. He said representatives from the Tesla company submitted a Site Plan Review application in September of this year. The applicants proposed to install electric vehicle (EV) charging stations within the existing parking lot at the Target store at 8005 Calumet Avenue.

HE said EV charging stations and infrastructure are not defined in the Munster zoning ordinance. The closest type of defined use is a 'gas station' which is permitted with a conditional use permit as a primary use in CD-4.A, CD-4.B, CD-5, and SD-M districts. EV charging stations are distinct, though, in that they can be installed within existing parking lots as an accessory use to a residential, commercial, civic, or manufacturing use and are not typically installed as primary uses.

He said that while not listed specifically as an accessory use in the CD-4.A, CD-4.B, CD-5, CZ, SD-PUD, and SD-M districts, each one of these districts contains a catch-all permitted accessory use: Other Uses that are Subordinate and Customarily Incidental to a Permitted Principal Use. He said he is requesting a determination whether EV charging stations are to be considered a use that is subordinate and customarily incidental to a permitted principal use in the CD-4.A, CD-4.B, CD-5, CZ, SD-PUD, and SD-M districts. This determination will establish whether EV charging stations are a permitted accessory use.

He said if the Board determines that EV charging stations are a permitted accessory use, a business would be permitted to install or permit another company to install EV charging stations within its parking lot provided that all development standards are otherwise met. These standards include: Minimum parking spaces, location of parking spaces, landscaping, site lighting, screening of utilities and infrastructure, and signage standards.

He said that if the Board determines that EV charging stations are not a permitted accessory use, then he will refer the matter to the Plan Commission for discussion whether to amend the zoning ordinance. They would be approved administratively through the building permit process.

Mr. Raffin said he wouldn't want generators in front of commercial buildings. Mr. Vander Woude said generators would be screened from the parking lot. In Munster, they would require that the utility infrastructure be screened in some form fashion and minimum required parking spaces. Ms. Mayer asked if a generator was connected to the station at the Town Hall. Mr. Vander Woude said it is not a generator, it is a transformer. Mr. Vander Woude said his recommendation is that this would be

considered an accessory use that would be typically associated with commercial uses. Some have already been installed. They don't seem to be disruptive. Mr. Raffin said research needs to be done to see what other municipal codes have regarding charging stations. Mr. Vander Woude said he could research and summarize his findings. Ms. Mayer said she thinks it could be considered an accessory use.

The MUNSTER BOARD OF ZONING APPEALS
MINUTES OF REGULAR BUSINESS MEETING
Meeting Date: March 14, 2023

The announced meeting location was Munster Town Hall and could be accessed remotely via Zoom, a video conference application.

Call to Order: 6:45 pm

Pledge of Allegiance

Members in Attendance:

Daniel Buksa
Sharon Mayer
Roland Raffin

Members Absent:

Brian Specht
Brad Hemingway

Staff Present:

Tom Vander Woude, Planning Director
Dave Wickland, Attorney

Mr. Buksa noted that the Board has a quorum, but action requires all three members present to vote the same.

Approval of Minutes:

Ms. Mayer requested a correction to the minutes. On page 4, 8 lines from the bottom, the word “size” should replace the word “sign”. The phrase should be “... reduce the size of the signs...”.

Motion: Ms. Mayer moved to approve the February 14, 2023, regular meeting minutes as amended.

Second: Mr. Raffin

Vote: Yes – 3 No – 0 Abstain – 0. Motion carries

Preliminary Hearings: None

Public Hearings:

Additional Business/Items for Discussion:

- a. Request for determination whether electric vehicle charging stations are a permitted accessory use in the CD-4.A, CD-4.B, CD-5, CZ, SD-PUD, and SD-M districts.**

Mr. Vander Woude said in November of 2022, he came before the Board of Zoning Appeals requesting direction on whether electric vehicle charging stations could be considered an accessory use in our business districts. While there is no specific reference to EV chargers in our zoning ordinance, there is a catch-all provision in all of our business districts that says, “any accessory use that is customarily found in this district is permitted”. The impetus for this was a proposal from the Tesla company to install EV charging stations in the Target parking lot. If the Board concurs that this use meets that provision, they can move forward with this particular project. Where there are questions of interpretation with respect to standards, the BZA can make a determination. At the November meeting, there was a discussion that the Munster zoning ordinance includes standards and regulations that relate to the screening of mechanical equipment, regulations for signs, and regulations for parking which we think would govern any EV charging station within another business's parking lot. For example, there is certain mechanical equipment that needs to be installed at some of these fast charging stations. Those would all be required to be screened under our ordinance. There are signage standards, we only permit directional signs of a certain size. Any parking spaces that would be set aside these EV chargers would have to be out of a surplus of parking on the lot for whatever is required for that use. The staff believes that those areas of concern would be addressed by our existing standards; they would be able to permit this type of thing in a commercial parking lot with little to no impact. Their position was that we should permit these as an accessory use. He said in November, the Board discussed some of these things and directed him to go out and research other communities are doing. He has compiled a number of resources that are listed in the staff report. He reviewed about five different guidebooks for how to implement EV charging in the community, how to use zoning to protect the community, and the best practices relating

to these types of uses. Every one of these resources said these should be permitted as accessory uses in business districts. There are some other standards in residential districts; you would permit some of these things in a business district that you would not in a residential district. For example, you wouldn't permit a Tesla charging station in someone's driveway. The best practice is to permit it to include standards that address screening, parking, and signage. In a best-case scenario, the zoning ordinance would have specific standards in dealing with EV chargers: a specific sign standard, screening standard and parking standard. In the absence of that in our code, there are already standards that address those issues. He also reviewed the Indiana State EV deployment plan, which stated that the policy in Indiana is to promote EV infrastructure so we can convert our police from gasoline to electric. The state of Indiana is actively planning for the deployment of EV infrastructure. They are looking for locations to install EV stations along the I-80 corridor. He also looked at a number of different communities in Indiana. There aren't many that regulate EV chargers specifically. He spoke to Sheila Shine, the Planning and Building director in Merrillville. They have something almost identical to this Tesla in the parking lot of their Meijer on US-30. Ms. Shine said Merrillville has never had any issues with traffic congestion or people loitering around the stations. The only issue they've ever had is that they planted some arborvitae around the utility compound, which were burned up by the heat from the compound and had to be replaced with a fence. He said Munster has standards in place for screening that permit a natural wood fence screen or masonry wall; PVC would not be permitted. Mr. Buksa asked who would own these charging stations. Mr. Vander Woude said, in this instance, the actual infrastructure is owned by the Tesla company, but the land underneath is owned by Target. The maintenance of the infrastructure is the responsibility of the Tesla company. Ms. Mayer said it is probably very similar to the windmill deals where they are leasing the land from farmers. Mr. Buksa asked who would be legally responsible for the stations. Mr. Vander Woude said Tesla would likely be legally responsible for the equipment. Mr. Buksa asked if there was an opportunity for Town revenue. Mr. Vander Woude said the Town would be free to install EV chargers on its own property and has done so in the Town Hall parking lot. There are other places in town where they are already installed at Community Hospital and in our parking lot, for example. Ms. Mayer asked about the different types 1, 2 and 3. If the power needed is available, why would they need to be handled differently. Mr. Vander Woude said it is not the power but rather is it the nature of how they it's used. If you have a Level 3, it charges much more quickly. Someone would be there for about 40 minutes rather than overnight. That is the biggest difference from a zoning standpoint. He doesn't know why they need different equipment on site for that. Ms. Mayer said she can see that because the power needs are so great, she doesn't understand the handling of it. Mr. Vander Woude said someone could put a Level 1 at their house and use it overnight. A Level 3 is more of a quick charger, so it has the potential for more frequent uses. His recollection from the reports that he's read are not necessarily prohibiting that but making it a conditional use in the residential district. That would allow the process to verify if it is only for the resident. Ms. Mayer said we are only looking at it only in Commercial districts. Mr. Vander said that our zoning ordinance currently includes that catch all provision in CD-4A, CD-4B and CD-5 districts. Mr. Raffin noted that the Town of Munster and Community Hospital are local people. He wants to make sure we have enforcement in place for a national brand, so we have protection to make sure they are safe. Mr. Vander Woude said all of our normal standards would apply. Whenever anything is done in Town, they are subject to the same standards with respect to maintenance and safety. They should have to license the business, so we have someone to call. Mr. Buksa said that should be a requirement. He asked if the Town's existing electrical infrastructure is sufficient to accommodate the EV chargers. Ms. Mayer answered that it was not

according to the plan. They have power on Calumet Avenue that they are bringing to that box inside the parking lot. Mr. Vander Woude said the only concern is that it should be brought in underground, and it couldn't be brought in overhead. Ms. Mayer said they show it underground on the plan. Mr. Vander Woude said he would like a motion confirming that EV charging stations are to be considered a use that is customarily in the district, subordinate to a permitted principle use.

Motion: Mr. Raffin moved to approve electric vehicle charging stations as a permitted accessory use in the CD-4.A, CD-4.B, CD-5, CZ, SD-PUD, and SD-M district but wants to make sure that are actual structures.

Second: Ms. Mayer, adding that the business must also be registered.

Vote: Yes – 3 No – 0 Abstain – 0. Motion carries.



BOARD OF ZONING APPEALS STAFF REPORT

To: Members of the Board of Zoning Appeals

From: Tom Vander Woude, Planning Director

Meeting Date: November 15, 2022

Agenda Item: N/A

Hearing: N/A

Application Type: Request for Interpretation

Summary: Request for determination whether electric vehicle charging stations are a permitted accessory use in the CD-4.A, CD-4.B, CD-5, CZ, SD-PUD, and SD-M districts.

BACKGROUND

SECTION 26-6.801 B. Referral to Board of Zoning Appeals for Interpretation states the following:

If the Zoning Administrator is in doubt as to the meaning or intent of any provision of this Article... such person shall refer the matter to the Board of Zoning Appeals for interpretation and decision.

Representatives from the Tesla company submitted a Site Plan Review application in September of this year. The applicants proposed to install electric vehicle (EV) charging stations within the existing parking lot at the Target store at 8005 Calumet Avenue. The plans are attached to this memo.

For reference, EV charging stations can be classified as follows:

1. Level 1 is considered slow charging and operates on a fifteen-to-twenty-amp breaker on a one hundred twenty-volt AC circuit.
2. Level 2 is considered medium charging and operated on a forty-to-one-hundred-amp breaker on a two hundred eight or two hundred forty-volt AC circuit.
3. Level 3 is considered fast or rapid charging and operated on a sixty amp or higher breaker on a four hundred eighty volt or higher three phase circuit with special grounding equipment. Level 3 stations can also be referred to as rapid charging stations that are typically characterized by industrial grade electrical outlets that allow for faster recharging of electric vehicles.

In this instance, the plans include both Level 3 Tesla Supercharger stations that can be used exclusively by Tesla vehicles and Level 2 charging stations that can be used by any electric vehicle.

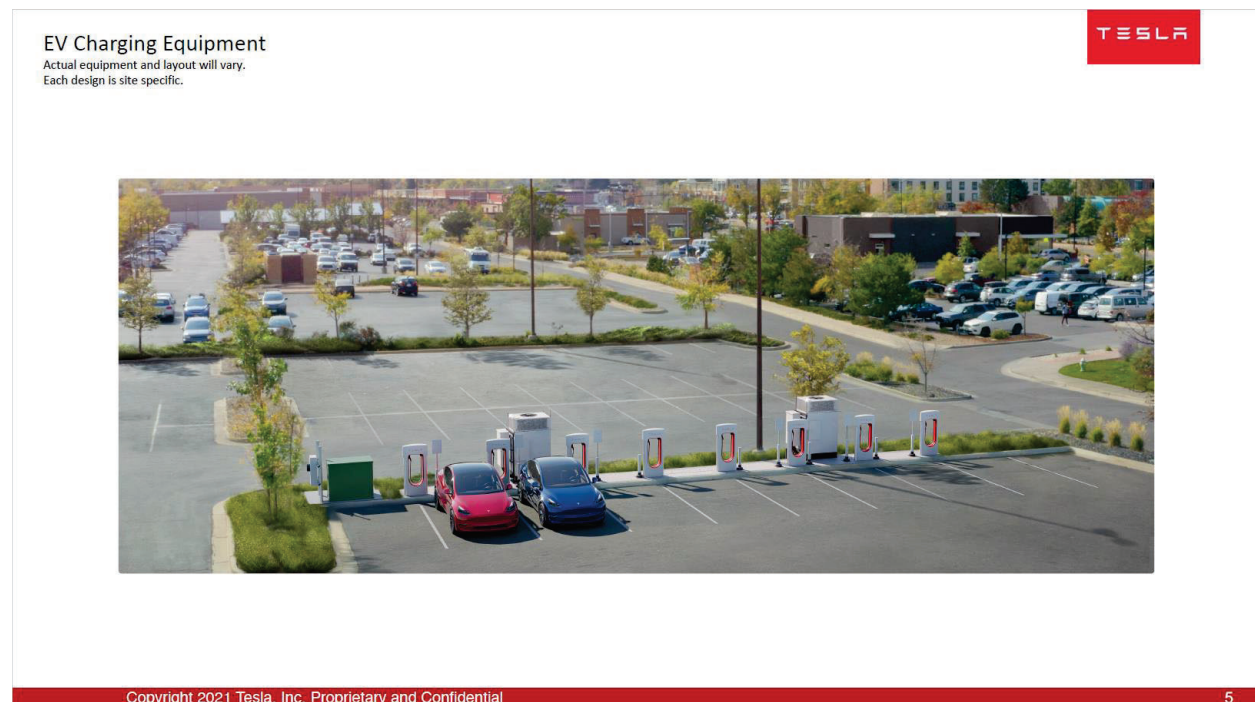


Figure 1 Example of EV charging stations in parking lot.



Figure 2 Example of EV charging stations in parking lot.



Figure 3 Example of EV charging stations in parking lot.

CURRENT ZONING

EV charging stations and infrastructure are not defined in the Munster zoning ordinance. The closest type of defined use is a 'gas station' which is permitted with a conditional use permit as a primary use in CD-4.A, CD-4.B, CD-5, and SD-M districts. EV charging stations are distinct, though, in that they can be installed within existing parking lots as an accessory use to a residential, commercial, civic, or manufacturing use and are not typically installed as primary uses.

While not listed specifically as an accessory use in the CD-4.A, CD-4.B, CD-5, CZ, SD-PUD, and SD-M districts, each one of these districts contains a catch-all permitted accessory use: *Other Uses that are Subordinate and Customarily Incidental to a Permitted Principal Use*.

Staff is requesting a determination whether EV charging stations are to be considered a *use that is subordinate and customarily incidental to a permitted principal use* in the CD-4.A, CD-4.B, CD-5, CZ, SD-PUD, and SD-M districts. This determination will establish whether EV charging stations are a permitted accessory use.

If the Board determines that EV charging stations are a permitted accessory use, a business would be permitted to install or permit another company to install EV charging stations within its parking lot provided that all development standards are otherwise met. These standards include:

- Minimum parking spaces
- Location of parking spaces
- Landscaping
- Site lighting

- Screening of utilities and infrastructure
- Signage standards

If the Board determines that EV charging stations are not a permitted accessory use, then I will refer the matter to the Plan Commission for discussion whether to amend the zoning ordinance.

RECOMMENDATION

Staff recommends discussion and a motion stating whether electric vehicle charging stations are to be considered a *use that is subordinate and customarily incidental to a permitted principal use* in the CD-4.A, CD-4.B, CD-5, CZ, SD-PUD, and SD-M districts.